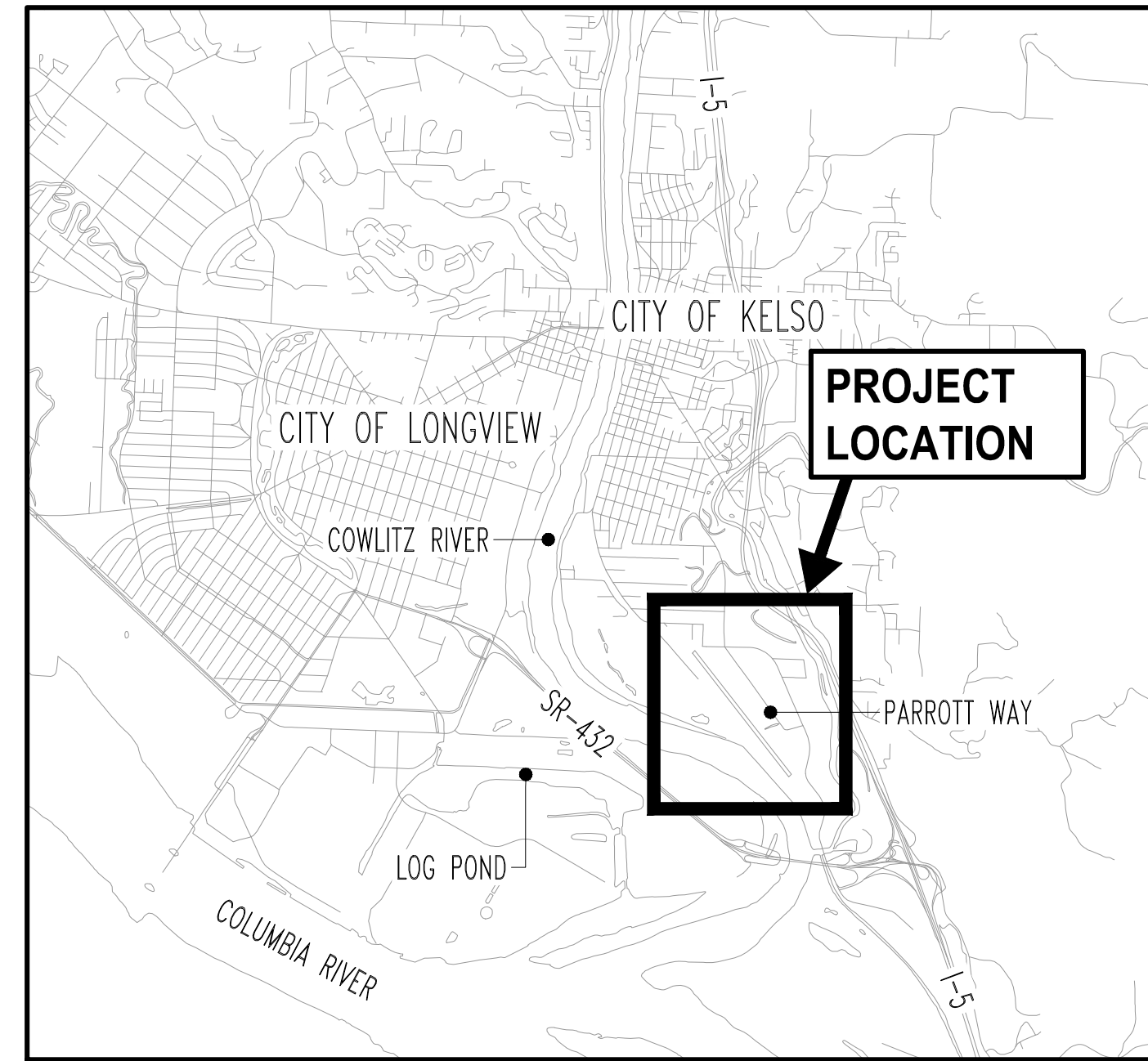


# CITY OF KELSO

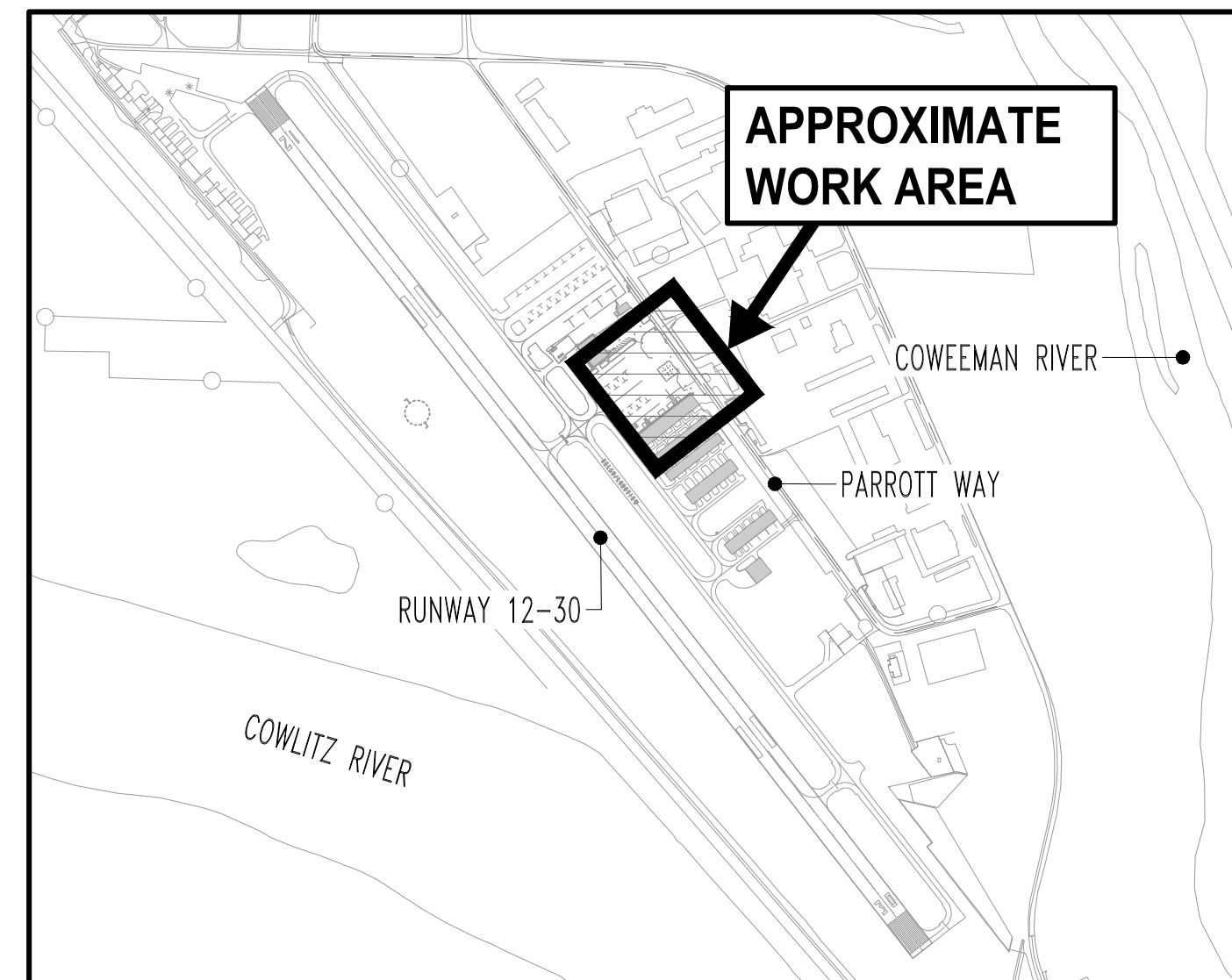
## SOUTHWEST WASHINGTON REGIONAL AIRPORT

### TASK ORDER #5 - FINAL DESIGN FUEL SITE

#### MAY 2023



**VICINITY MAP**  
NO SCALE



**SITE MAP**  
NO SCALE

SHEET INDEX		
SHEET NUMBER	DRAWING NUMBER	DRAWING TITLE
1	G-01	COVER SHEET
2	G-02	SITE AND SURVEY CONTROL PLAN
3	G-03	FUEL APRON SITE SAFETY & PHASING OVERVIEW PLAN
4	G-04	FUEL APRON SAFETY & PHASING NOTES
5	C-01	EROSION CONTROL PLAN
6	C-02	EROSION CONTROL NOTES & DETAILS
7	C-03	DEMOLITION PLAN 1 OF 2
8	C-04	DEMOLITION PLAN 2 OF 2
9	C-05	PAVING LAYOUT PLAN
10	C-06	PAVEMENT SECTIONS & DETAILS
11	C-07	GRADING & DRAINAGE PLAN
12	C-08	DRAINAGE DETAILS
13	C-09	PAVEMENT MARKING REMOVAL & REPLACEMENT PLAN
14	E-01	ELECTRICAL SYMBOL AND LEGEND
15	E-02	ELECTRICAL SITE PLAN
16	E-03	ELECTRICAL GROUNDING PLAN
17	E-04	ELECTRICAL ONE-LINE DIAGRAM AND SCHEDULES
18	E-05	ELECTRICAL DETAILS
19	E-06	ELECTRICAL WIRING DIAGRAMS
20	S1.0	STRUCTURAL NOTES
21	S2.0	MAT FOUNDATION
22	S2.1	MAT FOUNDATION

CONTACTS	
SOUTHWEST WASHINGTON REGIONAL AIRPORT AIRPORT MANAGER - CHRIS PAOLINI	
CENTURY WEST ENGINEERING PROJECT MANAGER - TRENT WARD, PE PROJECT CIVIL ENGINEER - DON BARCLAY, PE PROJECT ELECTRICAL ENGINEER - GRADY WEISZ, PE	

### LEGEND

— TSS —	EXISTING THRESHOLD SITING SURFACE
— RSA —	EXISTING RUNWAY SAFETY AREA
— ROFA —	EXISTING RUNWAY OBJECT FREE AREA
— ROFZ —	EXISTING RUNWAY OBSTACLE FREE ZONE
— RPZ —	EXISTING RUNWAY PROTECTION ZONE
— TSA —	EXISTING TAXIWAY SAFETY AREA
— TOFA —	EXISTING TAXIWAY OBJECT FREE AREA
	PROPOSED CONTRACTOR HAUL ROUTE
— x —	EXISTING CHAIN LINK FENCE
	PROPOSED SAWCUT
— -SD- -	EXISTING STORM DRAIN
— -W- -	EXISTING WATER LINE
— -P- -	EXISTING ELECTRICAL LINE
— -SS- -	EXISTING SANITARY SEWER LINE
— -T- -	EXISTING COMMUNICATION LINE
— -OP- -	EXISTING OVERHEAD POWER LINE
— -G- -	EXISTING GAS LINE
— ROW —	EXISTING RIGHT OF WAY/PROPERTY LINE
	EXISTING TELEPHONE HANDHOLE
	EXISTING POWER VAULT
	EXISTING TRANSFORMER
	EXISTING POWER POLE
	EXISTING WATER VAULT
	EXISTING CATCH BASIN
	EXISTING STORM DRAIN MANHOLE
	EXISTING SEWER MANHOLE
	EXISTING SIGN
	EXISTING CULVERT
	EXISTING WATER METER
	EXISTING POWER HANDHOLE

### ABBREVIATIONS

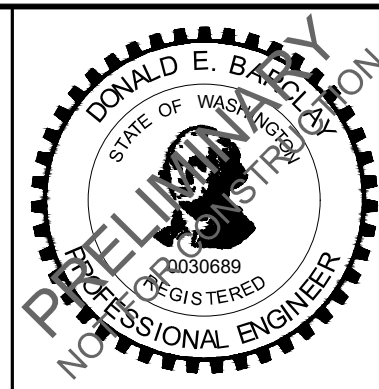
AASHTO	AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS
AC	ASPHALT CONCRETE PAVEMENT/ASBESTOS CONCRETE PIPE
AOA	AIRCRAFT OPERATIONS AREA
ASTM	AMERICAN SOCIETY FOR TESTING MATERIALS
B/N	BETWEEN
BO	BLOWOFF
BOW	BACK OF WALK
CB	CATCH BASIN
CDBG	COMMUNITY DEVELOPMENT BLOCK GRANT
CL	CENTERLINE
CMP	CORRUGATED METAL PIPE
CO	CLEANOUT
CONC	CONCRETE
CSBC	CRUSHED SURFACING BASE COURSE
CSTC	CRUSHED SURFACING TOP COURSE
DI	DUCTILE IRON
DIA	DIAMETER
EA	EACH
ECY	DEPARTMENT OF ECOLOGY
EL	ELEVATION
EOP	EDGE OF PAVEMENT
ESC	EROSION AND SEDIMENT CONTROL
EX	EXISTING
EXST	EXISTING
FG	FINISHED GRADE
FH	FIRE HYDRANT
FL	FLANGE/FLOW LINE
FO	FIBER OPTIC
FOW	FACE OF WALK
FT	FOOT/FEET
GALV	GALVANIZED
HDPE	HIGH DENSITY POLYETHYLENE
HMA	HOT MIX ASPHALT
HYD	HYDRANT
IE	INVERT ELEVATION
I&I	INFLOW & INFILTRATION
IN	INCHES
MAX	MAXIMUM
MH	MANHOLE
MIN	MINIMUM

### ABBREVIATIONS CONT.

MJ	MECHANICAL JOINT
NPT	NATIONAL PIPE THREAD
OC	ON CENTER
OFF	OFFSET
OSHA	OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION
P	POWER
PACS	PRIMARY AIRPORT CONTROL STATION
T	TELECOMMUNICATIONS
TLN	TAXILANE
TW	TAXIWAY
TWY	TAXIWAY
PCC	PORTLAND CEMENT CONCRETE
PE	PLAIN END
PROP	PROPOSED
PVC	POLY VINYL CHLORIDE
ROW	RIGHT OF WAY
RPR	RESIDENT PROJECT REPRESENTATIVE
RW	RUNWAY
RWY	RUNWAY
SACS	SECONDARY AIRPORT CONTROL STATION
SCH	SCHEDULE
SS	SANITARY SEWER
SD	STORM DRAIN
STA	STATION
TBC	TOP BACK OF CURB
TESC	TEMPORARY EROSION & SEDIMENT CONTROL
TYP	TYPICAL
W	WATER
WISHA	WASHINGTON INDUSTRIAL SAFETY AND HEALTH ACT
WS	WATER SERVICE
WSDOE	WASHINGTON STATE DEPARTMENT OF ECOLOGY
WSDOT	WASHINGTON STATE DEPARTMENT OF TRANSPORTATION

### PERMIT SET

C:\chris\CenturyWest\Sound\FuelSite\KELSO\_CITY\_OF\_SWOVA\_2023\FuelSite\TO #5 Final Design Fuel Site\G-01 COVER SHEET.dwg



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509.639.2710 FAX

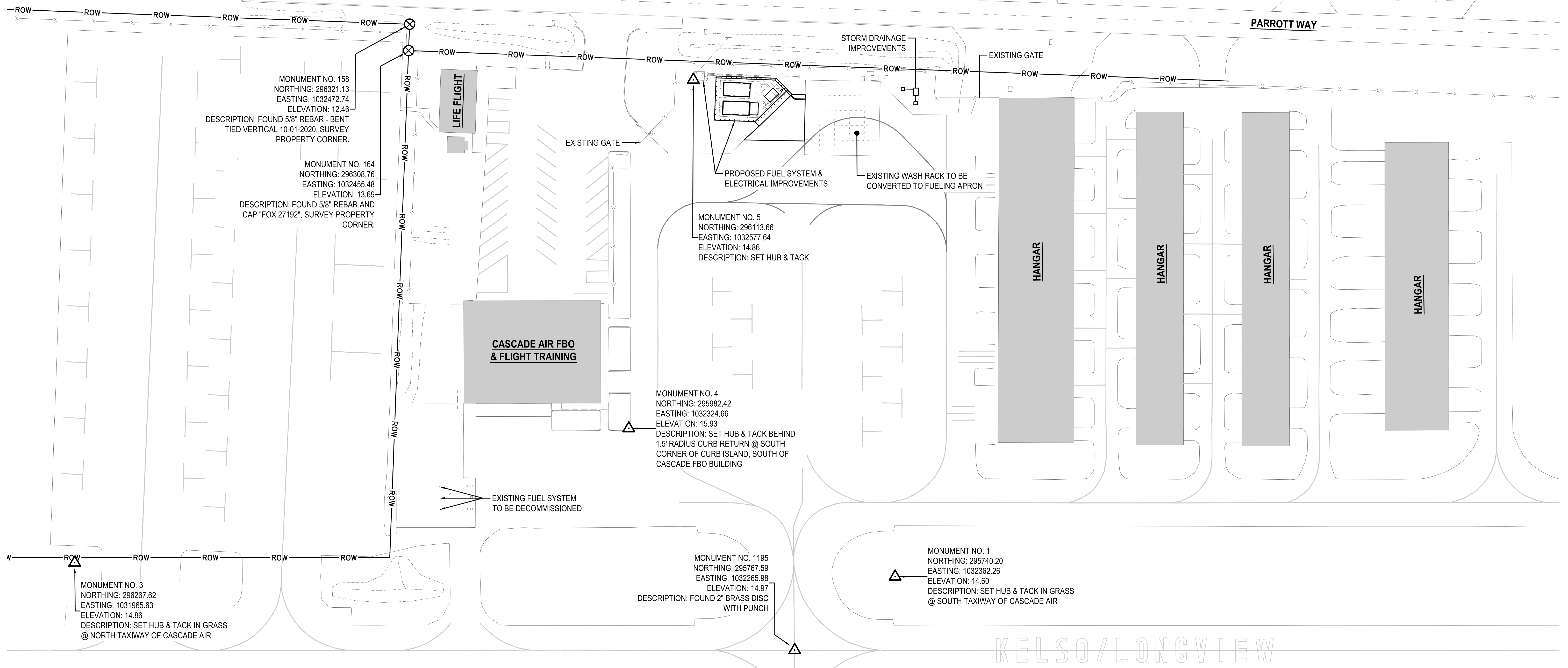
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SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

COVER SHEET

DRAWING NO. G-01
SHEET NO. 1 OF 22





**LEGEND**

- SURVEY CONTROL POINT
- CORNER MONUMENT
- EXISTING PROPERTY LINE/RIGHT-OF-WAY

**SURVEY NOTES**

1. HORIZONTAL AND VERTICAL CONTROL SURVEY REFERENCE POINTS ARE PROVIDED FOR CONTROL OF THE PROJECT BASED ON TOPOGRAPHIC SURVEY COMPLETED BY GIBBS AND OLSON ON MARCH 1, 2022 AND SUPPLEMENTED ON FEBRUARY 1, 2023. ALL LAYOUT AND CONSTRUCTION SURVEYING SHALL BE CONDUCTED BY A COMPANY UNDER THE SUPERVISION OF A PROFESSIONAL LAND SURVEYOR REGISTERED IN WASHINGTON.
2. PRIOR TO BEGINNING ANY LAYOUT, THE CONTRACTOR'S SURVEYOR SHALL OCCUPY ALL REFERENCE CONTROL POINTS SHOWN ON THIS SHEET AND VERIFY DATA GIVEN. ANY DISCREPANCY SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEER'S ATTENTION FOR CLARIFICATION OR CORRECTION. NOTES SHOWING CONFIRMATION OF THE HORIZONTAL AND VERTICAL IN ACCORDANCE WITH THE GENERAL CONTRACT PROVISIONS SHALL BE PROVIDED TO THE ENGINEER PRIOR TO STARTING CONSTRUCTION.

**SURVEY NOTES CONT.**

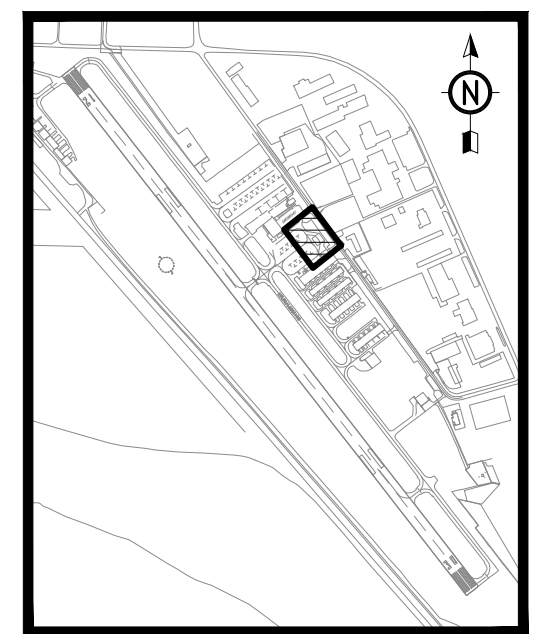
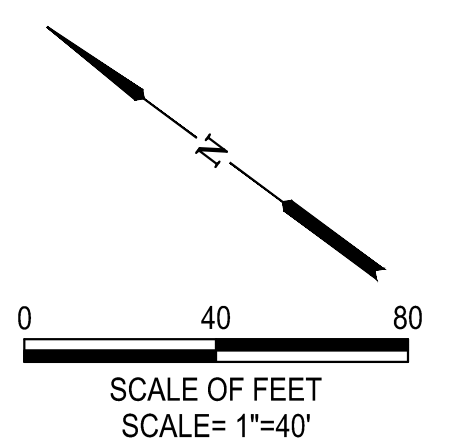
3. WORK PERFORMED USING A 2 SECOND TOPCON GT-502 ROBOTIC TOTAL STATION, TOPCON HIPER-VR GPS-RTK EQUIPMENT, PRECISION EXCEEDS REQUIREMENTS OF W.A.C. 332-130-090
4. THE PURPOSE OF THIS TOPOGRAPHIC SURVEY IS FOR CIVIL ENGINEERING AND ARCHITECTURAL DESIGN. THIS IS NOT A BOUNDARY SURVEY.
5. MT. VIEW LOCATING SERVICE, LLC, (PO BOX 40, SUMNER, WA 98390), WAS CONTRACTED TO MARK UTILITIES IN SELECT AREAS AS DIRECTED BY GIBBS AND OLSON, (1157 3RD AVENUE, LONGVIEW, WA 98632).  
WASHINGTON UTILITY NOTIFICATION CENTER MUST BE NOTIFIED AT 800-424-5555 OR 811 PRIOR TO ANY CONSTRUCTION OR UNDERGROUND UTILITY LOCATION.  
UTILITY LOCATIONS ARE GATHERED BY MEASUREMENTS TO SURFACE MARKS AND LOCATION PAINT PROVIDED BY MT. VIEW LOCATING SERVICES, LLC IN THE FIELD. SURVEYOR MAKES NO GUARANTEE OF THE UNDERGROUND UTILITIES SHOWN IN THE AREA. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED.
6. EXISTING SITE FEATURE PORTRAYED AS BACKGROUND INFORMATION ON ALL PLANS IN THIS DRAWING SET ARE BASED ON THE BEST INFORMATION AVAILABLE AND WERE COMPILED USING INFORMATION OBTAINED FROM RECORD DRAWINGS PROVIDED BY THE OWNER, GIS, AND THE TOPOGRAPHIC SURVEY COMPLETED BY GIBBS AND OLSON ON MARCH 1, 2022 AND SUPPLEMENTED ON FEBRUARY 1, 2023. CONTRACTOR SHALL VERIFY ALL EXISTING SITE CONDITIONS IN THE FIELD AND NOTIFY THE ENGINEER IMMEDIATELY IF THERE ARE DISCREPANCIES TO WHAT IS SHOWN ON THESE PLANS.

**BASIS OF BEARING:**

N36°56'38"W BETWEEN MONUMENTS NO. 1 AND NO. 3, WASHINGTON PLANE COORDINATE SYSTEM (WPCS), SOUTH ZONE 4602, NAD 83/2011 (EPOCH: 2010), GEIOD: G2012BU1, NAVD88, U.S. FEET, DERIVED FROM WASHINGTON STATE REFERENCE NETWORK (WSRN) STATION: CROK3

MONUMENT NO. 1 N: 295740.20 E: 1032362.26 ELEV: 14.60 LAT: N046° 07' 08.3175" LON: W122° 53' 53.5079"	MONUMENT NO. 3 N: 296267.62 E: 1031965.63 ELEV: 14.86 LAT: N046° 07' 13.4028" LON: W122° 53' 59.3638"
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UNLESS OTHERWISE NOTED, DISTANCES ARE GROUND DISTANCES.  
TO CALCULATE GRID DISTANCE, MULTIPLY GROUND DISTANCE BY THE COMBINED SCALE FACTOR: 0.99994679

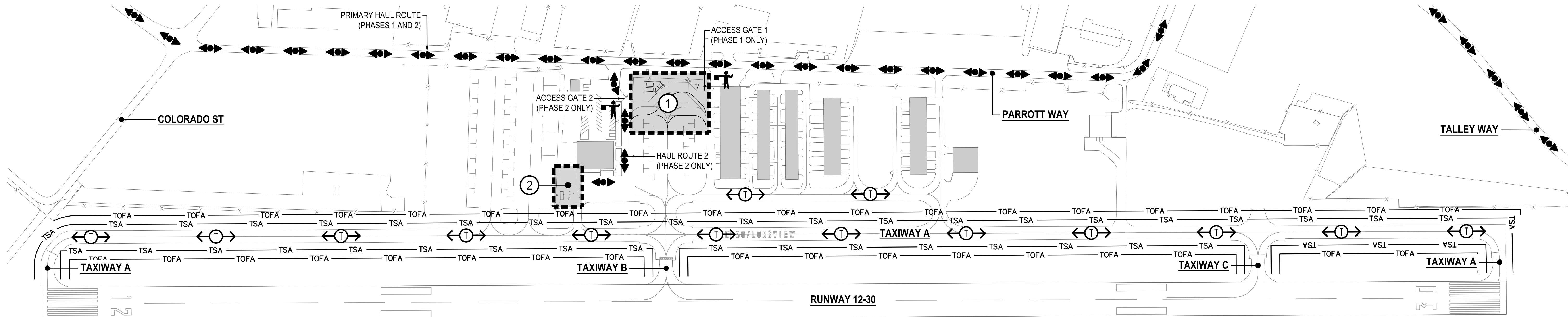


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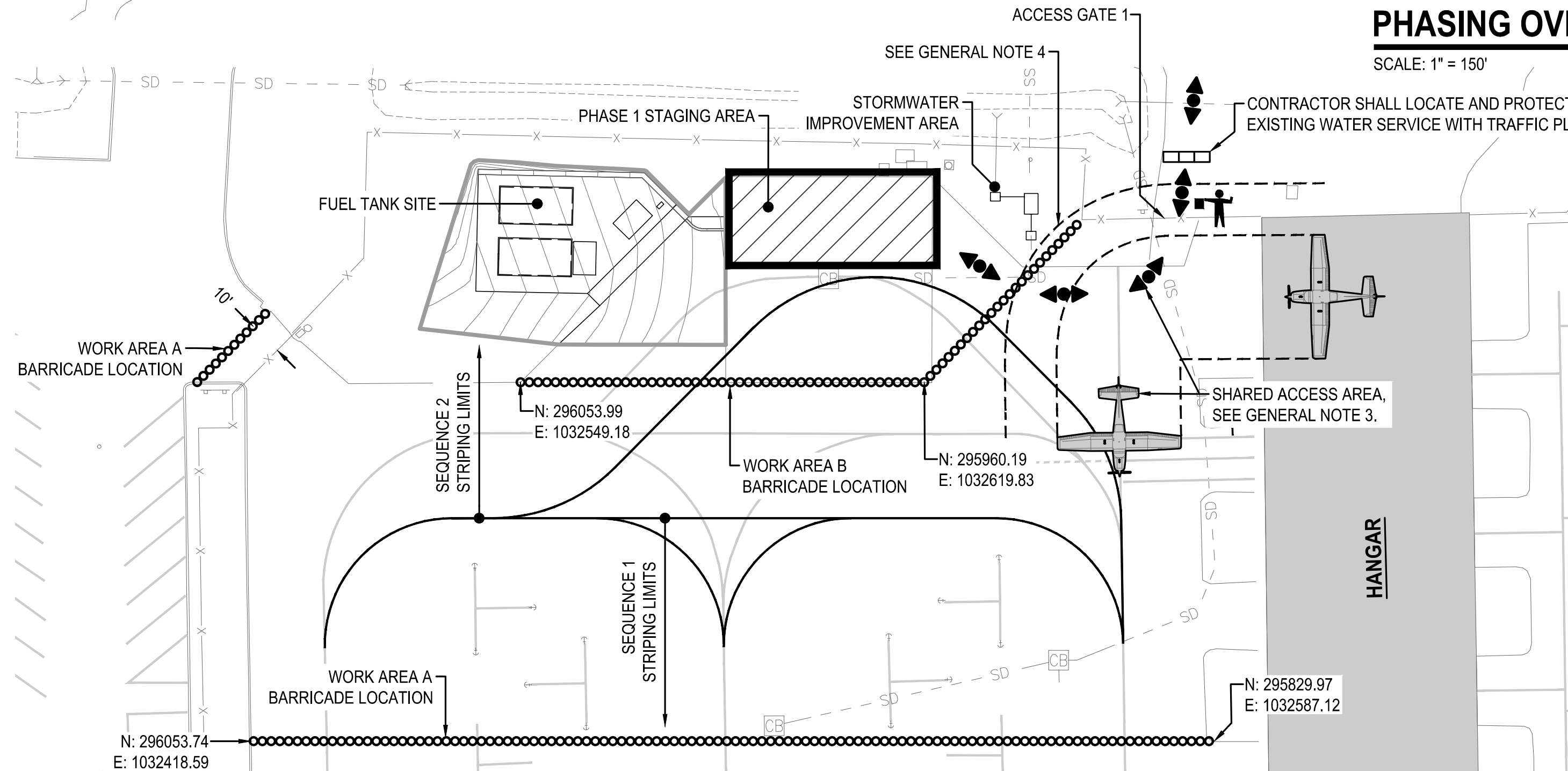
			<p><b>VERIFY SCALES</b> BAR IS ONE INCH ON ORIGINAL DRAWING. 0" = 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>APPR</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DATE	BY	APPR	REVISIONS																<p>ELLENSBURG OFFICE 208 W 9TH AVENUE, SUITE 3 ELLENSBURG, WA 98926 509.795.5870 509.639.2710 FAX</p>	<p>DESIGNED BY: DMY DRAWN BY: JCW CHECKED BY: DEB SCALE: AS NOTED</p>	<p>SOUTHWEST WASHINGTON REGIONAL AIRPORT TASK ORDER #5 - FINAL DESIGN FUEL SITE</p>	<p>DRAWING NO. <b>G-02</b></p>
NO.	DATE	BY	APPR	REVISIONS																								
<p>SITE AND SURVEY CONTROL PLAN</p>							<p>SHEET NO. <b>2 OF 22</b></p>																					

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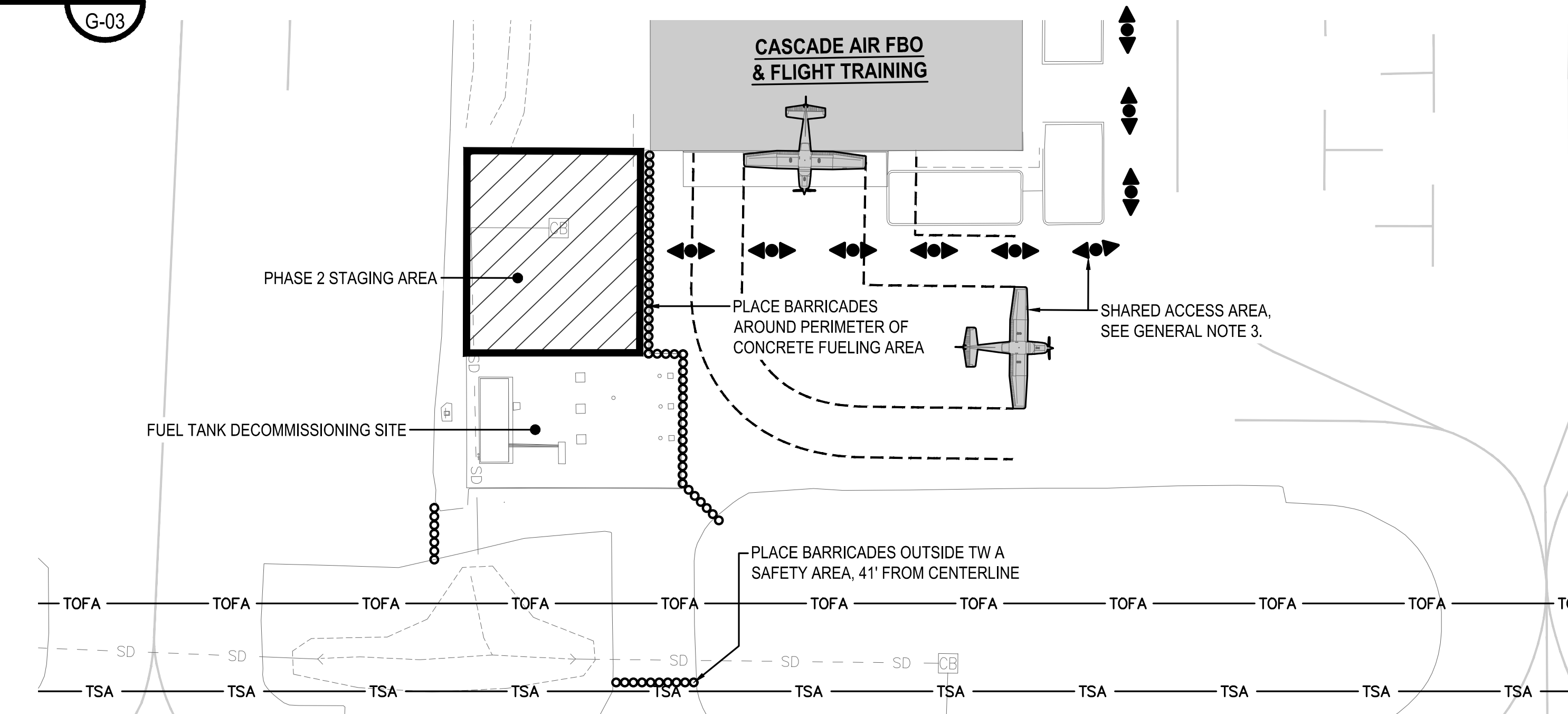




**PHASING OVERVIEW** 1  
SCALE: 1" = 150'  
G-03



**PHASE 1 ENLARGED PLAN** 2  
SCALE: 1" = 30'  
G-03



**PHASE 2 ENLARGED PLAN** 3  
SCALE: 1" = 30'  
G-03

**LEGEND**

- PROPOSED CONTRACTOR STAGING AREA BY PHASE
- CONTRACTOR ACCESS AND HAUL ROUTE
- AOA SECURITY GUARD
- AIRCRAFT TAXI ROUTE THIS PHASE
- LOW PROFILE BARRICADE

**LEGEND CONT.**

- TSA TAXIWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA

- GENERAL NOTES**
- ALL WORK SHALL BE SEQUENTIAL AS NOTED IN THE "CONSTRUCTION PHASING & OPERATION CONSTRAINTS" TABLE. CONCURRENT WORK SHALL NOT BE ALLOWED.
  - REFER TO SHEET C-07 FOR ADDITIONAL NOTES AND REQUIREMENTS.
  - AREAS WHERE ACCESS/HAUL ROUTES CROSS ACTIVE AIRCRAFT AREAS AND IN FRONT OF EXISTING HANGARS SHALL BE CONTINUOUSLY MONITORED FOR FOD. THE CONTRACTOR SHALL NOT, UNDER ANY CIRCUMSTANCE, BLOCK ACCESS TO THESE HANGARS.
  - STORM DRAINAGE WORK CONFLICTS WITH ACCESS TO HANGAR NEAREST SITE ACCESS GATE. CONTRACTOR SHALL PULL ALL PERSONNEL AND EQUIPMENT BACK 5 FEET FROM BARRICADE LINE, WHEN NEEDED, TO ALLOW AIRCRAFT TO PASS.

**CONSTRUCTION PHASING & OPERATION CONSTRAINTS**

PROJECT PHASE	DURATION	WORK AREA	SEQUENCE	WORK SCOPE	RUNWAY STATUS			
					RW 12-30	TW A	TW B	TW C
1	2 HOURS	A	1	- STRIPING REMOVALS - TIE-DOWN REMOVALS - PARTIAL RESTRIPING AS SHOWN	OPEN	OPEN	OPEN	OPEN
	154 CALENDAR DAYS	B	2	- FUEL TANK SITE DEVELOPMENT - STORMWATER IMPROVEMENTS				
	2 HOURS	A	3	- FINAL RESTRIPING AS SHOWN				
2	28 CALENDAR DAYS	N/A	4	- EXISTING FUEL TANK DECOMMISSIONING - EXISTING FUEL BUILDING DEMOLITION	OPEN	OPEN	OPEN	OPEN

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509.639.2710 FAX

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SCALE: AS NOTED

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

FUEL APRON SITE SAFETY & PHASING OVERVIEW PLAN

DRAWING NO. G-03  
SHEET NO. 3 OF 22



**GENERAL WORK AREA NOTES:**

1. THE CONTRACTOR SHALL BE RESTRICTED TO USE THE ENTRANCE AND HAULING ROUTES SHOWN ON THE DRAWINGS. FOLLOW AIRPORT AND FAA SAFETY PROCEDURES WHEN MOVING EQUIPMENT OR PERSONNEL. NO PERSONAL VEHICLES SHALL BE ALLOWED OUTSIDE OF THE STAGING AREA. THE AIRPORT MAY IMMEDIATELY REMOVE ANY PERSONNEL AND EQUIPMENT FROM THE SITE IN VIOLATION OF AIRPORT SAFETY AND SECURITY PROCEDURES.
2. COVER GUIDANCE SIGNS AS DIRECTED BY THE AIRPORT TO PREVENT AIRCRAFT FROM TAXIING TOWARDS CLOSED WORK AREAS.
3. LIMIT EQUIPMENT HEIGHT TO 20 FEET UNLESS OTHERWISE APPROVED BY THE ENGINEER.
4. LIMIT STOCK PILES TO 20 FEET IN HEIGHT AND AT LEAST 250 FEET FROM RUNWAY CENTERLINE AND 66 FEET FROM TAXIWAY CENTERLINE.
5. IN THE EVENT OF AN EMERGENCY, MOVE ALL EQUIPMENT AND PERSONNEL TO THE CONTRACTOR'S STAGING AREA UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
6. ACCESS TO ANY WORK AREA MUST BE AUTHORIZED BY THE ENGINEER PRIOR TO WORK IN THAT AREA. NOTIFY THE ENGINEER A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK IN ANY AREA. NOTAMS SHALL BE ISSUED BY THE AIRPORT.
7. PLACE LOW LEVEL BARRICADES AS SHOWN AND DESCRIBED IN SECTION 01300 OF THE SPECIFICATIONS.
8. BARRICADE SHALL BE PLACED PRIOR TO BEGINNING CONSTRUCTION OPERATIONS AND SHALL BE REMOVED WHEN TAXILANES ARE REOPENED.
9. RETURN ALL EQUIPMENT TO STAGING AREA AT THE END OF EACH WORK DAY.
10. FOR ADDITIONAL REQUIREMENTS, REFER TO SECTION 01330, AIRPORT SAFETY, IN THE TECHNICAL SPECIFICATIONS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE TO LOCATE AND PROTECT ALL UTILITIES DURING THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING THE UTILITY NOTIFICATION CENTER TO LOCATE PUBLIC UTILITIES AND FOR ANY ADDITIONAL UTILITY LOCATES INCLUDING HIRING A PRIVATE LOCATE SERVICE IF REQUIRED. ANY UTILITIES DAMAGED IN CONJUNCTION WITH THE CONSTRUCTION ACTIVITIES SHALL BE REPLACED AND/OR REPAIRED BY THE CONTRACTOR AS APPROVED BY THE OWNER.
12. ALL HAUL ROUTES SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT. VERIFY CONDITION WITH OWNER PRIOR TO CONSTRUCTION.

**WORK IN SAFETY AREAS AND OBJECT FREE AREAS:**

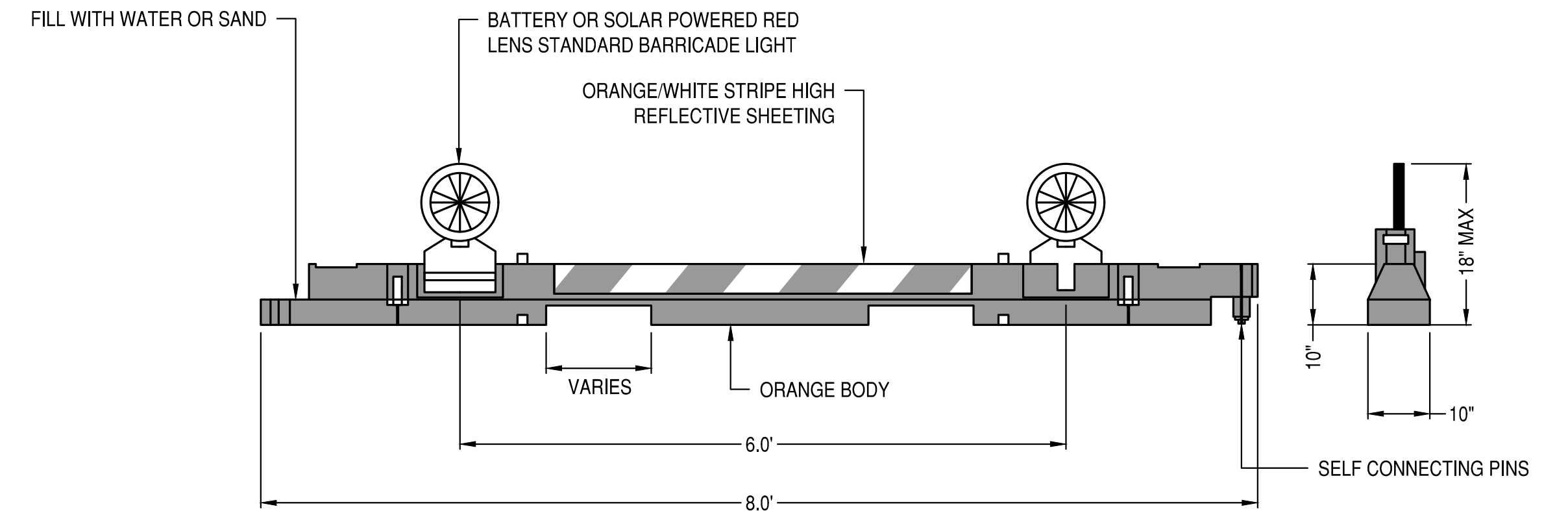
1. NO WORK IS ALLOWED IN ACTIVE RUNWAY AND TAXIWAY SAFETY AREAS. RUNWAY SAFETY AREA (RSA) AND TAXIWAY SAFETY AREA (TSA) ARE DEFINED AS AREAS THAT SHALL BE CLEARED AND GRADED AND HAVE NO RUTS, HUMPS, OPEN TRENCHES, DEPRESSIONS, OR OTHER SURFACE VARIATIONS. THE MAXIMUM SLOPE ANYWHERE WITHIN A RUNWAY OR TAXIWAY SAFETY AREA SHALL BE 3%. IN TRANSITIONS FROM PAVED TO UNPAVED AREAS, A 1.5 INCH VERTICAL DROP IS ALLOWED. THE RUNWAY AND TAXIWAY SAFETY AREAS SHALL BE MAINTAINED AT ALL TIMES WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC. PERSONNEL, EQUIPMENT, OR MATERIAL WITHIN A RUNWAY SAFETY AREA AT ANY TIME REQUIRES A CLOSURE. SAFETY AREAS THAT ARE CLOSED FOR CONSTRUCTION MUST MEET THESE CRITERIA PRIOR TO REOPENING.
2. THE CONTRACTOR SHALL ANTICIPATE THE CONSTRUCTION OF TEMPORARY FILLS, COMPACTION, TRENCH BACKFILLING, AND GRADING TO MEET THE REQUIREMENTS OF "WORK IN SAFETY AREAS AND OBJECT FREE AREAS", PRIOR TO REOPENING RUNWAYS AND TAXIWAYS. THIS WORK IS CONSIDERED INCIDENTAL TO VARIOUS WORK ITEMS AND SEPARATE PAYMENT WILL NOT BE MADE.
3. CONTRACTOR SHALL NOT ENTER ANY ACTIVE RSA OR TSA WITHOUT AUTHORIZATION FROM THE AIRPORT. ALL EQUIPMENT, TOOLS, AND MATERIALS SHALL BE MOVED TO STAGING AREAS PRIOR TO REOPENING A RUNWAY OR TAXIWAY.
4. SAFETY AREA LIMITS  
 TAXIWAY - 39.5' FROM FROM CENTERLINE  
 RUNWAY 12-30 - 75' FROM CENTERLINE; 300' FROM THRESHOLD
5. RUNWAY AND TAXIWAY OBJECT FREE AREAS (OFA) ARE DEFINED AS AREAS THAT SHALL BE CLEAR OF FIXED OR MOVABLE OBJECTS. EQUIPMENT NOT IN USE AND MATERIAL STOCKPILES AND STORAGE SHALL BE PLACED AT LEAST 250 FEET FROM ANY RUNWAY CENTERLINE. NO WORK MAY OCCUR IN TAXIWAY OBJECT FREE AREAS UNLESS THE TAXIWAY IS CLOSED TO AIRCRAFT TRAFFIC. NO STORAGE OF EQUIPMENT, MATERIALS OR STOCKPILES IS ALLOWED WITHIN TAXIWAY OBJECT FREE AREAS.
6. OBJECT FREE AREA LIMITS  
 RUNWAY 12-30 - 250' FROM CENTERLINE  
 TAXIWAYS - 65.5' FROM CENTERLINE
7. RUNWAY AND TAXIWAY SAFETY AREA AND RUNWAY AND TAXIWAY OBJECT FREE AREA CRITERIA MUST BOTH BE MET PRIOR TO OPENING AN IMPACTED RUNWAY OR TAXIWAY.

**SEQUENCING AND WORK SCHEDULE NOTES:**

1. COORDINATE WORK AREA CLOSURES WITH THE AIRPORT TO ENSURE ACCESS AS NEEDED TO MAINTAIN AIRPORT OPERATIONS, INCLUDING PROVIDING FOR AND MAINTAINING ACCESS TO THE FOLLOWING:
  - ACCESS TO THE APRON AREAS
  - ACCESS TO THE FBO RAMP
2. REFER TO SHEET G-03 FOR PHASE DURATIONS, WORK SCOPE AND SEQUENCING.
3. UNLESS NOTED OTHERWISE, WORK MAY BE 24/7 AT THE CONTRACTOR'S OPTION.

**CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICE:**

1. THE CONTRACTOR SHALL HAVE A FULL TIME SUPERINTENDENT ON THE PROJECT SITE ANY TIME WORK IS BEING ACCOMPLISHED ON THE AIRPORT. THIS INCLUDES WORK BEING PERFORMED BY SUBCONTRACTORS OR SERVICE PROVIDERS UNLESS OTHERWISE APPROVED BY THE OWNER.
2. THE CONTRACTOR SHALL ASSIGN ONE PERSON TO BE THE SAFETY OFFICER DURING THE PROJECT. THE SAFETY OFFICER SHALL BE ONSITE AT ALL TIMES WHEN WORK IS OCCURRING IN THE AOA WHEN THE AIRPORT IS OPEN UNLESS OTHERWISE APPROVED BY THE OWNER. THE SAFETY OFFICER SHALL BE RESPONSIBLE FOR THE EDUCATION AND TRAINING OF ALL PERSONNEL WHO WILL ACCESS AIRPORT PROPERTY. THE OFFICER SHALL MAINTAIN RECORDS INDICATING THAT PERSONNEL HAVE BEEN TRAINED AND ARE FAMILIAR WITH SAFETY RULES AND REGULATIONS RELATED TO OPERATIONS AT THE AIRPORT DURING CONSTRUCTION. PERSONNEL WHO VIOLATE SAFETY REQUIREMENTS MAY BE REMOVED FROM THE PROJECT AT THE DISCRETION OF THE OWNER. THE SAFETY OFFICER SHALL ALSO BE RESPONSIBLE FOR FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT.
3. THE CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER SHALL ATTEND THE WEEKLY CONSTRUCTION MEETING.

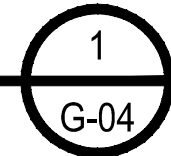


**NOTES:**

1. BARRICADES SHALL MEET THE REQUIREMENTS OF AC 150/5370-2, CURRENT EDITION.
2. PLACE BARRICADE PER THE PLANS OR AS DIRECTED BY THE RPR.
3. BARRICADES SHALL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING, FILLING, EMPTYING, MOVING, MAINTAINING AND PROTECTING BARRICADES THROUGHOUT THE DURATION OF THE PROJECT.
4. LIGHTS ARE TO BE NO FARTHER APART THAN 10' WHEN PLACED.
5. BARRICADES SHALL BE COVERED WITH REFLECTIVE SHEETING OR OTHER MATERIAL APPROVED BY THE RPR.
6. BARRICADES SHALL BE APPROPRIATELY WEIGHTED DOWN TO RESTRICT MOVEMENT FROM HIGH WINDS OR PROP WASH.

**LOW LEVEL BARRICADE DETAIL**

SCALE=NTS



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**PERMIT SET**

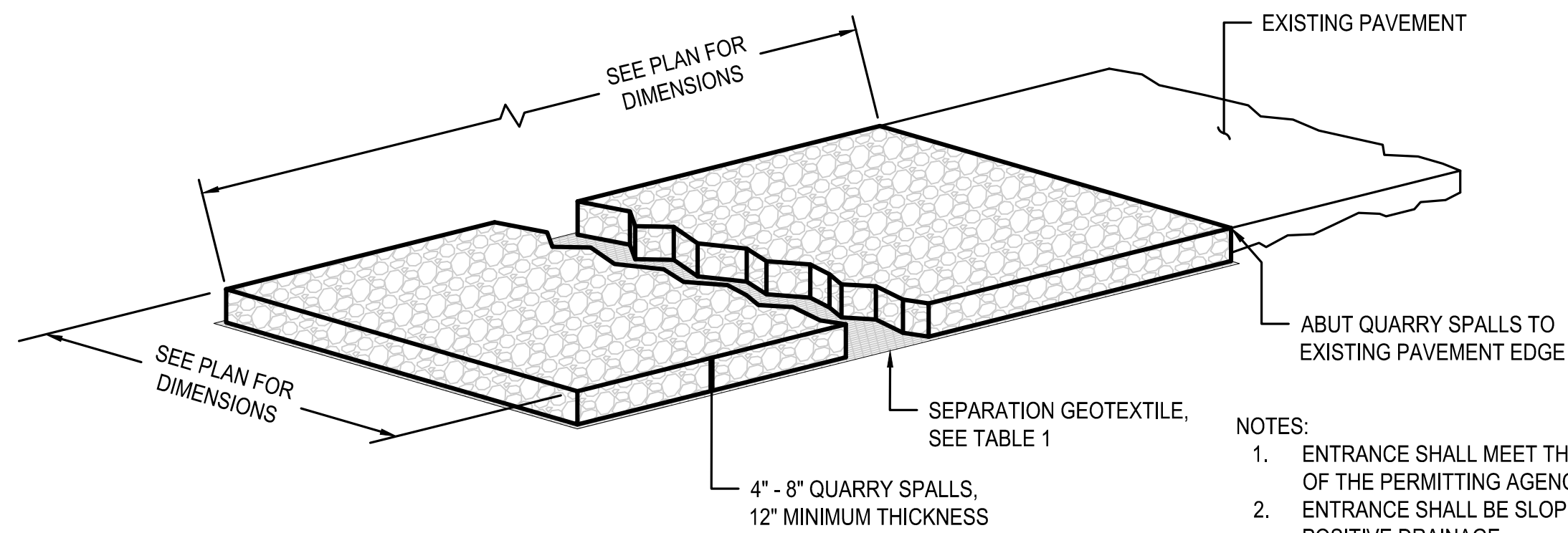
			<p><b>VERIFY SCALES</b>                  BAR IS ONE INCH ON ORIGINAL DRAWING.                  0" = 1"                  IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>APPR</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DATE	BY	APPR	REVISIONS																										<p>ELLENBURG OFFICE                  208 W 9TH AVENUE, SUITE 3                  ELLENBURG, WA 98926                  509.795.5870                  509.639.2710 FAX</p>	<p>DESIGNED BY: DMY</p> <p>DRAWN BY: DMY</p> <p>CHECKED BY: DEB</p> <p>SCALE: AS NOTED</p>	<p><b>SOUTHWEST WASHINGTON REGIONAL AIRPORT                  TASK ORDER #5 - FINAL DESIGN FUEL SITE</b></p> <p><b>FUEL APRON SAFETY &amp; PHASING NOTES</b></p>	<p>DRAWING NO. <b>G-04</b></p> <p>SHEET NO. <b>4 OF 22</b></p>
NO.	DATE	BY	APPR	REVISIONS																																		
					DATE: MAY 2023	PROJECT NO: 35005.010.02																																







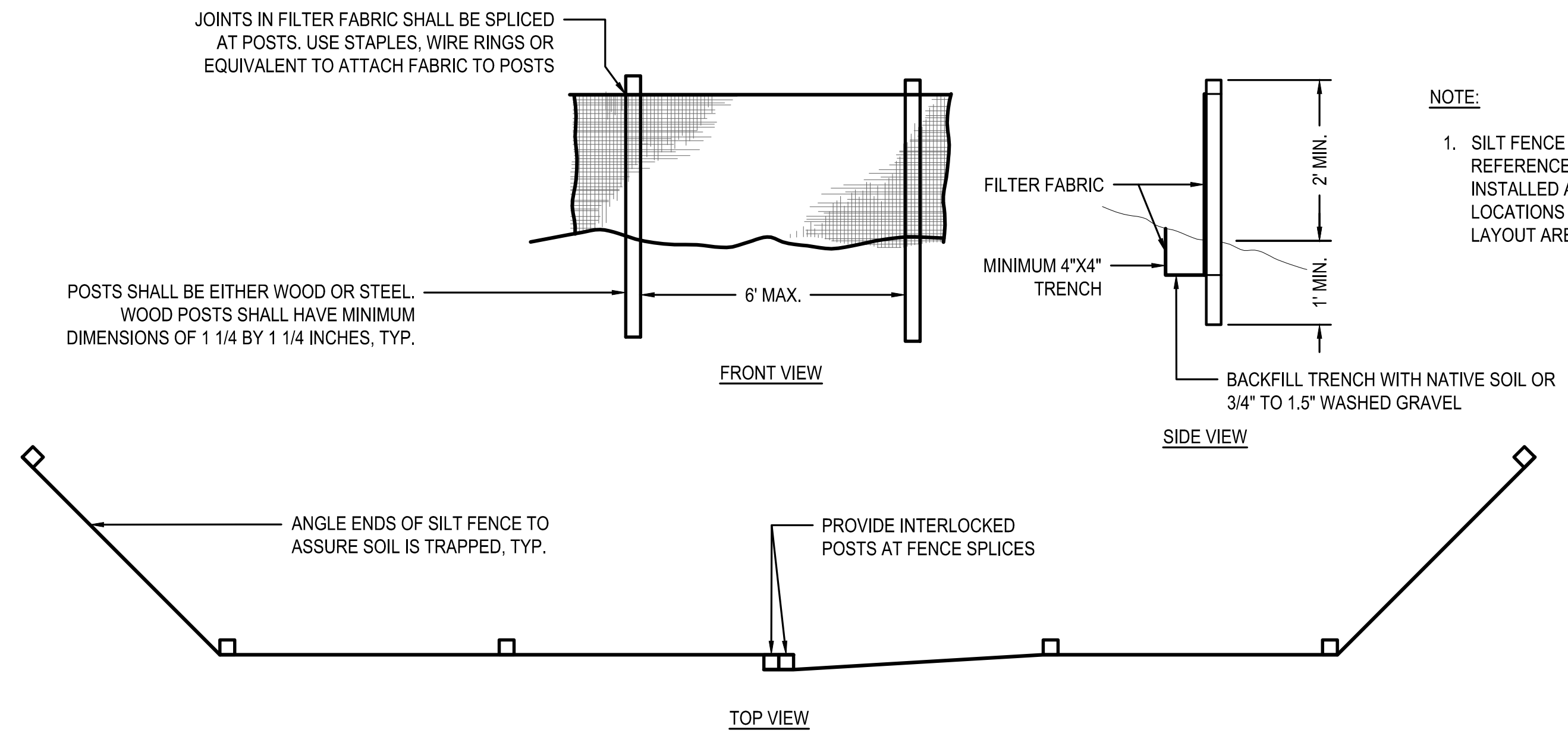
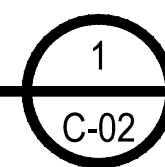
TABLE 1 - GEOTEXTILE STANDARDS	
GRAB TENSILE STRENGTH (ASTM D4751)	200 PSI MIN.
GRAB TENSILE ELONGATION (ASTM D4632)	30% MAX.
MULLEN BURST STRENGTH (ASTM D3786-80a)	400 PSI MIN.
AOS (ASTM D4751)	20-45 (US STANDARD SIEVE SIZE)



- NOTES:
- ENTRANCE SHALL MEET THE REQUIREMENTS OF THE PERMITTING AGENCY.
  - ENTRANCE SHALL BE SLOPED TO PROMOTE POSITIVE DRAINAGE.
  - THE ENTRANCE SHALL BE REMOVED AND RESTORED TO ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. RESTORATION SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION ENTRANCE.

### STABILIZED CONSTRUCTION ENTRANCE

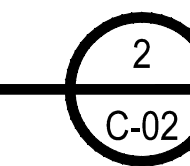
SCALE=NTS



- NOTE:
- SILT FENCE DETAIL IS PROVIDED FOR REFERENCE. SILT FENCE SHALL BE INSTALLED AS NEEDED. APPROXIMATE LOCATIONS FOR INITIAL SILT FENCE LAYOUT ARE PROVIDED ON THE PLANS.

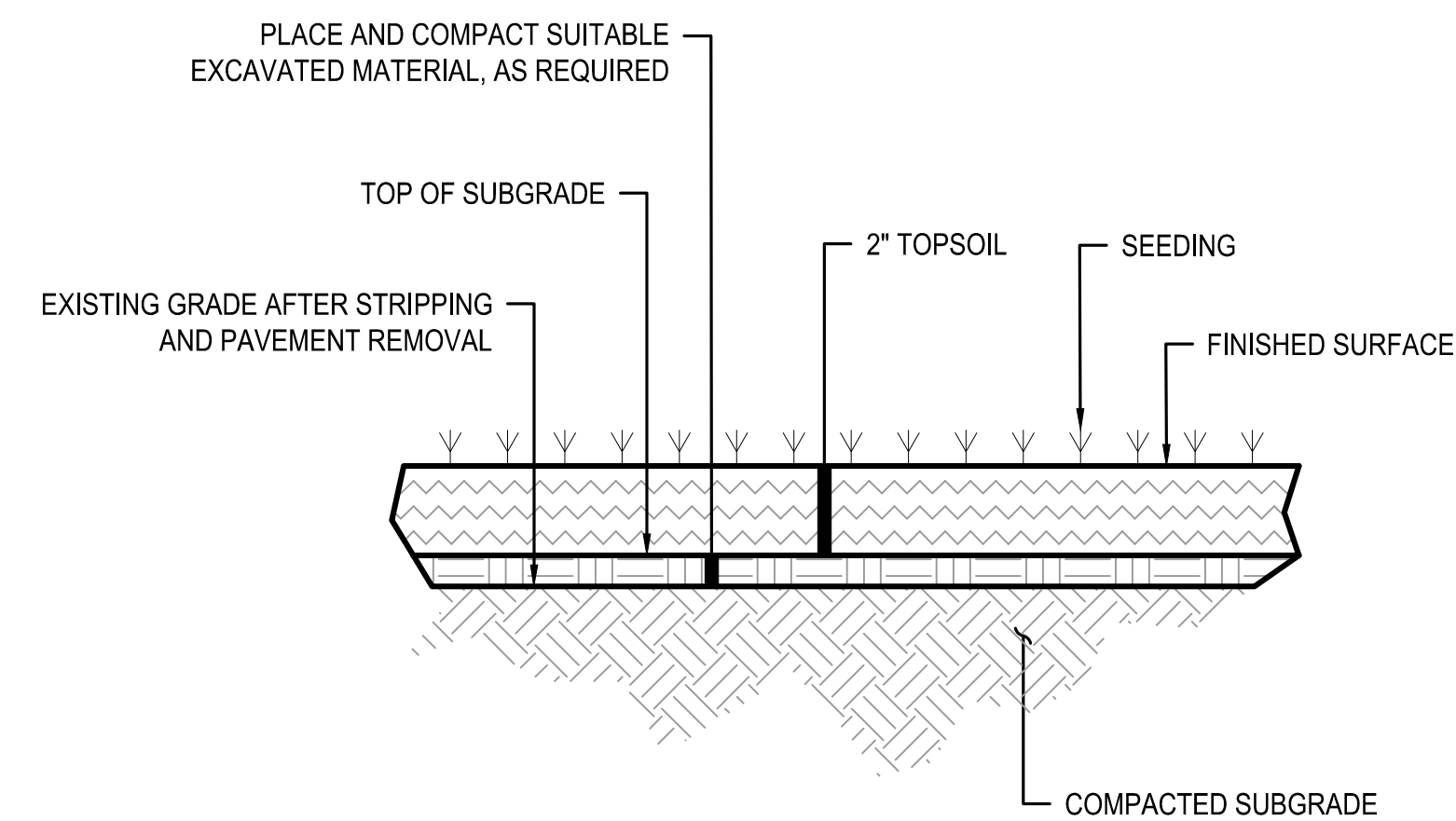
### SILT FENCE

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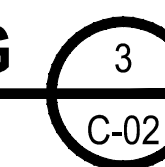
### EROSION CONTROL GENERAL NOTES

- ALL EROSION CONTROL DEVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THIS EROSION CONTROL PLAN AND THE EROSION CONTROL DETAILS, AND IN PLACE PRIOR TO THE START OF ANY LAND DISTURBING ACTIVITY.
- ALL EROSION PREVENTION AND CONTROL BMP'S SHALL BE INSPECTED, MAINTAINED AND REPAIRED AS NEEDED THROUGHOUT CONSTRUCTION TO ENSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION
- PERFORM ALL ACTIONS NECESSARY TO PREVENT EROSION AND CONTROL SEDIMENT, INCLUDING DUST, FROM LEAVING THE CONSTRUCTION SITE.
- AS CONSTRUCTION PROGRESSES AND SEASONAL CONDITIONS DICTATE, MORE EROSION AND SEDIMENT CONTROLS MAY BE REQUIRED TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATERS DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR SURFACE WATERS.

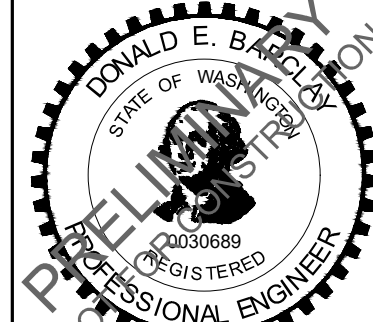


### TYPICAL TOPSOIL & SEEDING

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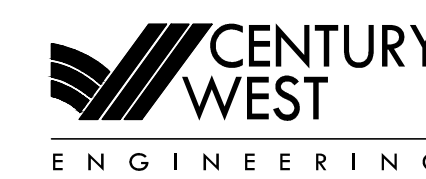


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ELLENSBURG OFFICE  
208 W 9TH AVENUE, SUITE 3  
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509.795.5870  
509.639.2710 FAX

DESIGNED BY:  
DMY  
DRAWN BY:  
JCW  
CHECKED BY:  
DEB  
SCALE:  
AS NOTED

DATE:  
MAY 2023

PROJECT NO:  
35005.010.02

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

EROSION CONTROL NOTES & DETAILS

DRAWING NO.  
C-02

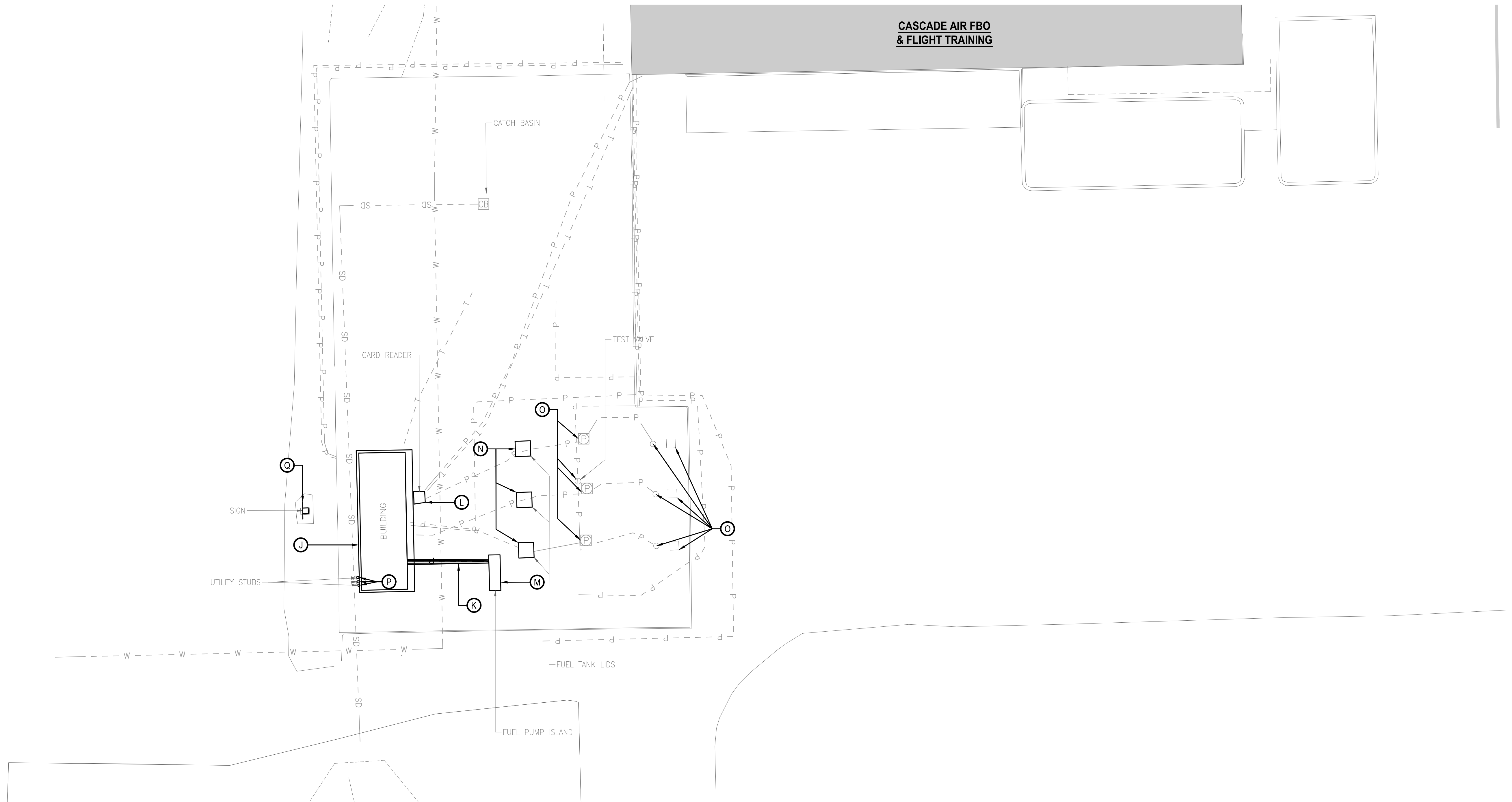
SHEET NO.  
6 OF 22







CASCADE AIR FBO  
& FLIGHT TRAINING



**LEGEND**

- PAVEMENT SAWCUT LINE, SEE GENERAL NOTE 3
- PROPOSED STRIPPING LIMITS, SEE GENERAL NOTE 4
- HMA MILLING, 1/4 INCH DEPTH
- HMA MILLING TRANSITION, DEPTH VARIES 1/4 INCH TO 1 INCH, AS SHOWN
- 1/4-IN 1/4 INCH MILLING DEPTH BOUNDARY
- 1-IN 1 INCH MILLING DEPTH BOUNDARY

**GENERAL NOTES**

1. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. DAMAGES SHALL BE REPAIRED BY THE AFFECTED UTILITY AT THE CONTRACTOR'S EXPENSE.
2. UNLESS NOTED OTHERWISE, EXISTING INFRASTRUCTURE TO REMAIN SHALL BE PROTECTED.
3. SAWCUTTING OF HMA PAVEMENT SHALL BE INCIDENTAL TO PAVEMENT REMOVAL BID ITEM AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE.

**GENERAL NOTES CONT.**

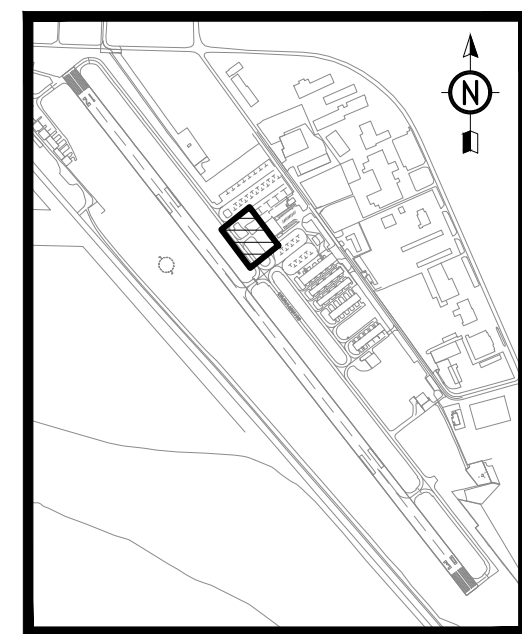
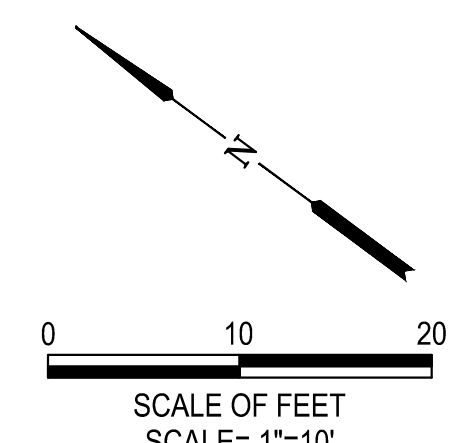
4. EXISTING VEGETATED AREAS WITHIN THE GRADING LIMITS SHALL BE STRIPPED TO A DEPTH OF 2-4 INCHES.
5. FOR PAVEMENT MARKING REMOVALS, SEE SHEET C-09.

**KEYED NOTES**

- DEMOLISH EXISTING BUILDING. HAZARDOUS MATERIAL SHALL BE ABATED IN ACCORDANCE WITH THE SPECIFICATIONS.
- REMOVE EXISTING EXTRUDED CONCRETE CURB WITH EMBEDDED ELECTRICAL CONDUIT.
- REMOVE EXISTING CARD READER AND PULL POWER AND COMMUNICATIONS LINES BACK TO PANEL. CHIP OUT AND GRIND EXISTING HOUSEKEEPING PAD FLUSH.
- REMOVE EXISTING FUEL PUMP ISLAND AND ALL ASSOCIATED APPURTENANCES. PULL POWER CONDUCTORS BACK TO PANEL.

**KEYED NOTES CONT.**

- REMOVE AND DISPOSE OF EXISTING FUEL. FILL TANKS WITH CDF IN ACCORDANCE WITH SPECIFICATIONS. REMOVE EXISTING RAISED LID CASTINGS AND BACKFILL OPENING WITH CDF TO 6" BELOW THE SURFACE. FILL THE FINAL 6" WITH CONCRETE AND FINISH SURFACE SMOOTH.
- REMOVE EXISTING COVER AND FILL OPENING WITH CDF TO 6" BELOW THE SURFACE. FILL THE FINAL 6" WITH CONCRETE AND FINISH SURFACE SMOOTH. ABANDON EXISTING FRAME IN PLACE.
- REMOVE CONDUCTORS BACK TO PANEL. CUT OFF AND PLUG RISERS FLUSH WITH GRADE.
- REMOVE AND DISPOSE OF EXISTING SIGN



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ELLENSBURG OFFICE  
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ELLENSBURG, WA 98926  
509.795.5870  
509.639.2710 FAX

DATE: MAY 2023 PROJECT NO: 35005.010.02

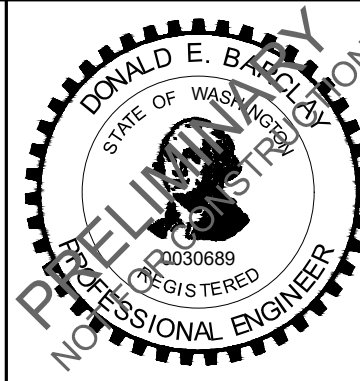
DESIGNED BY: DMY  
DRAWN BY: JCW  
CHECKED BY: DEB  
SCALE: AS NOTED

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

DEMOLITION PLAN 2 OF 2

DRAWING NO. C-04  
SHEET NO. 8 OF 22

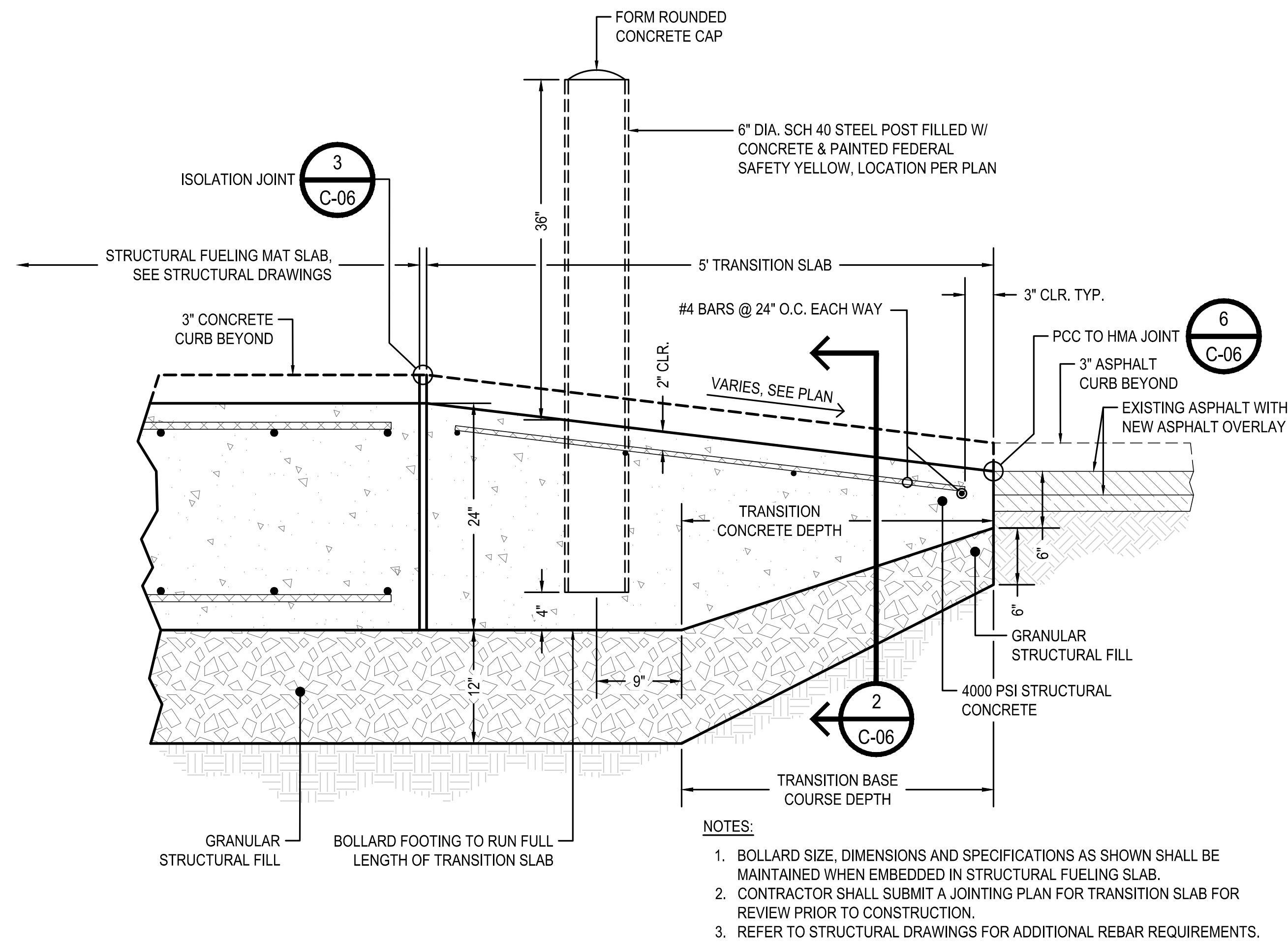
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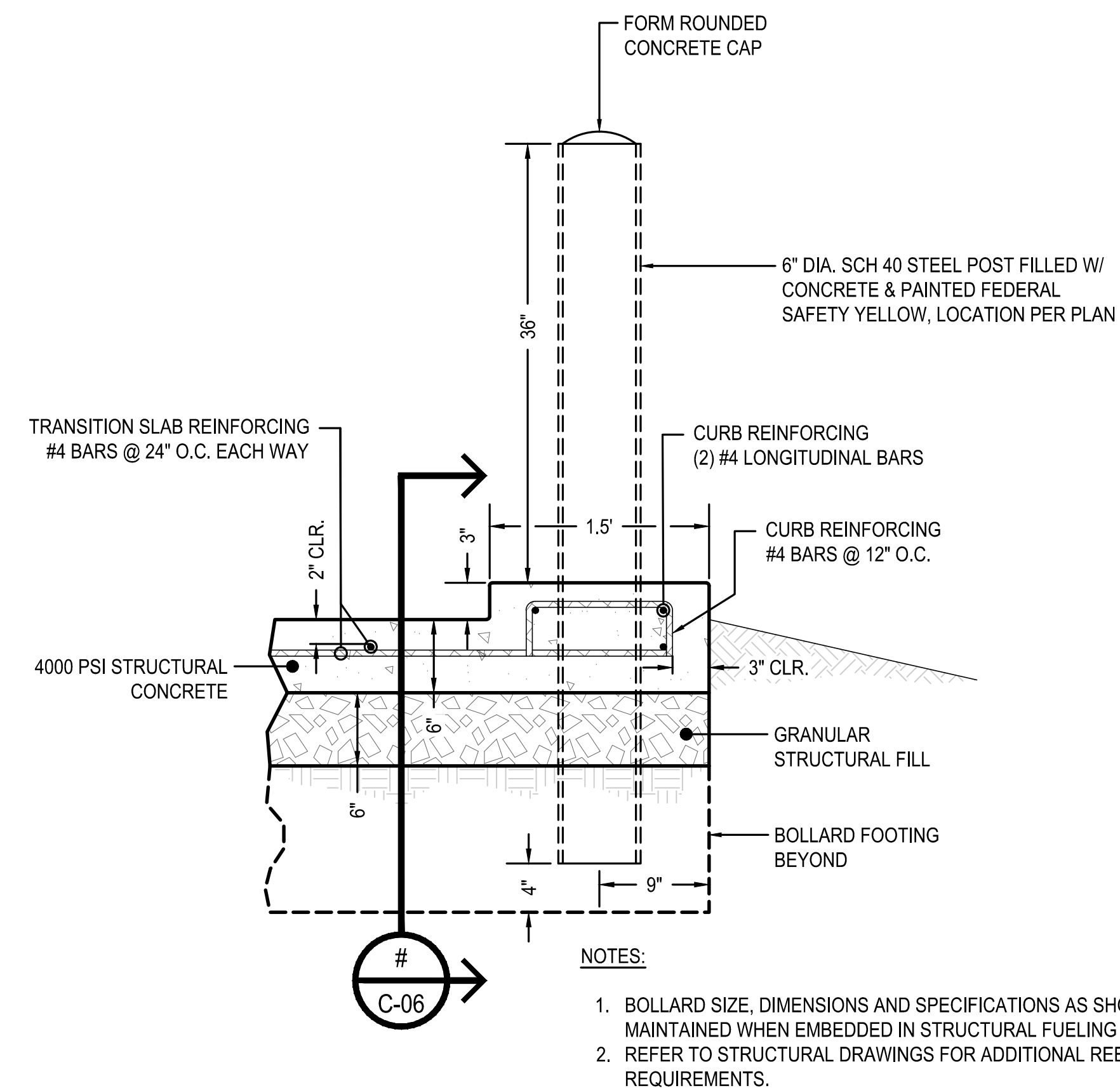




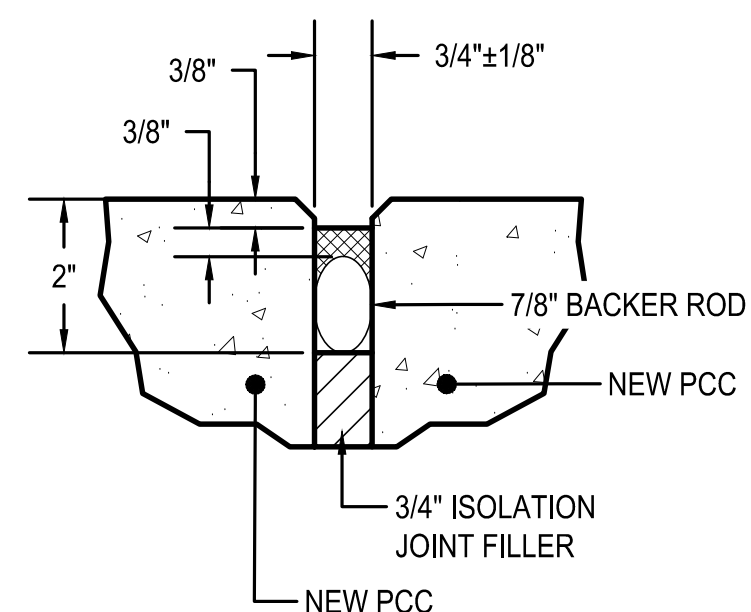




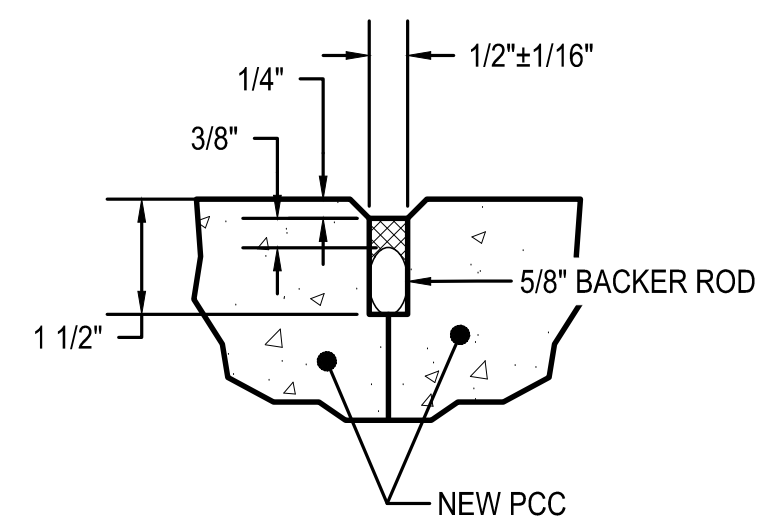
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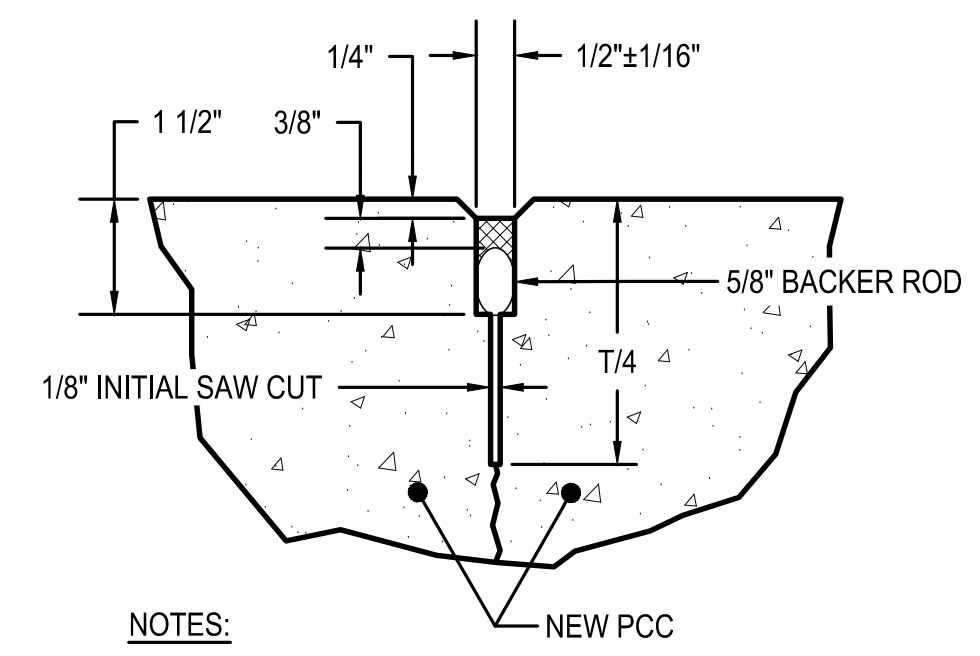
**TRANSITION SLAB CURB** 2 C-06  
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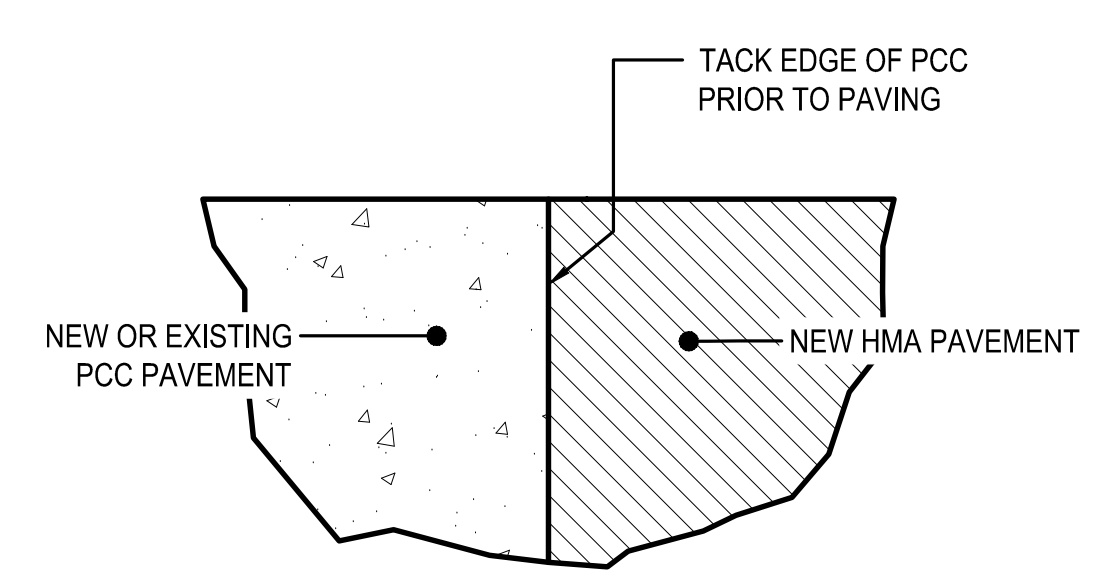
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**CONSTRUCTION JOINT** 4 C-06  
SCALE=NTS



**CONTRACTION JOINT** 5 C-06  
SCALE=NTS

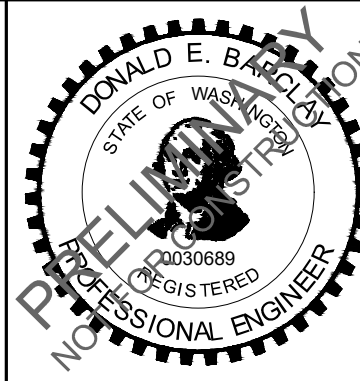


**PCC TO HMA JOINT** 6 C-06  
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**JOINT SEAL NOTES:**

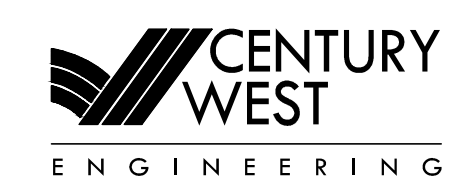
- JOINT DEPTH, SEAL BEAD THICKNESS AND SEAL RECESS DIMENSION ARE APPROXIMATE AND SHALL BE ADJUSTED AS REQUIRED TO MEET JOINT SEAL MANUFACTURER'S RECOMMENDED DIMENSIONS.
- ALL PAVEMENT JOINTS SHALL BE CONSIDERED INCIDENTAL TO THE APPLICABLE HMA PAVING AND CONCRETE PLACEMENT BID ITEMS.

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ELLENSBURG, WA 98926  
509.795.5870  
509.639.2710 FAX

DATE: MAY 2023 PROJECT NO: 35005.010.02

DESIGNED BY: DMY  
DRAWN BY: JCW  
CHECKED BY: DEB  
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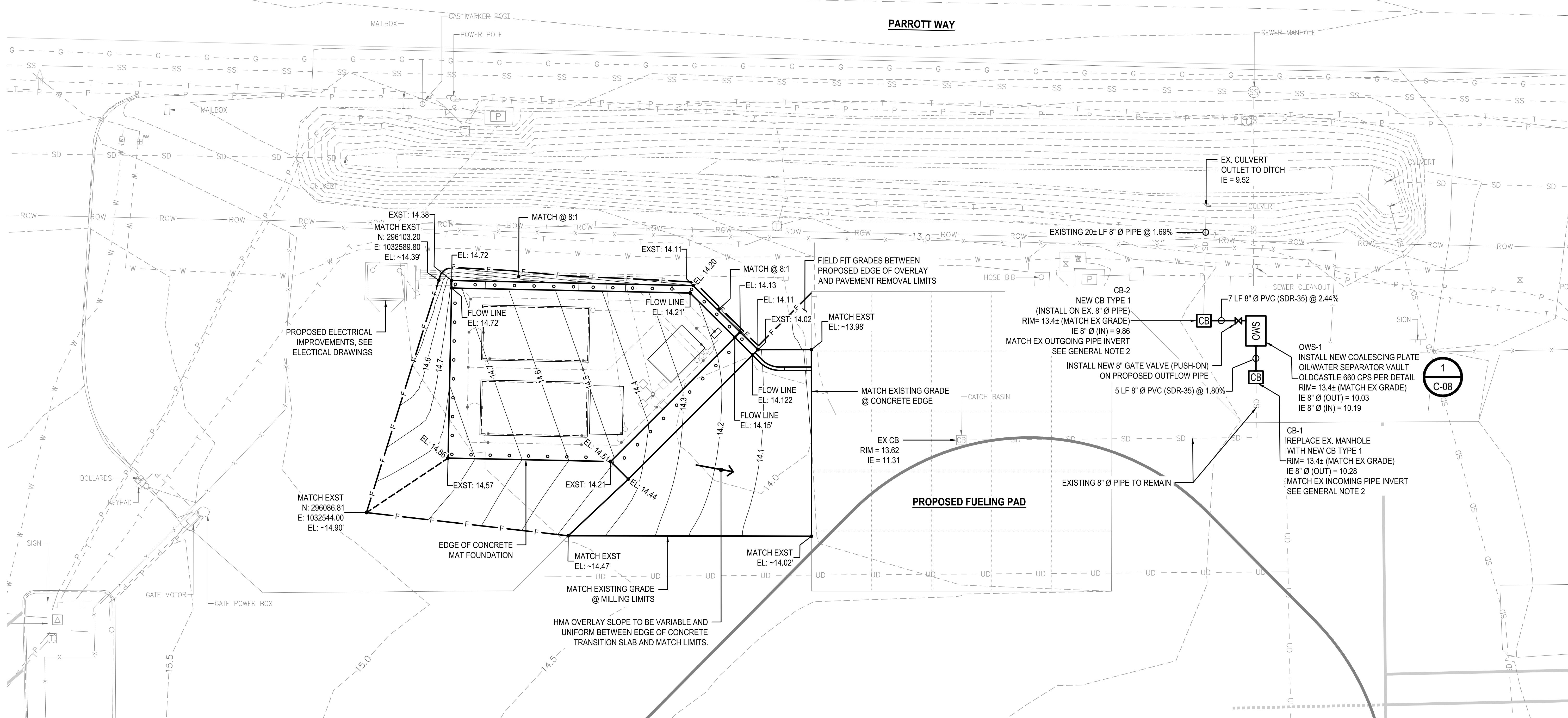
SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

PAVEMENT SECTIONS & DETAILS

DRAWING NO. C-06  
SHEET NO. 10 OF 22

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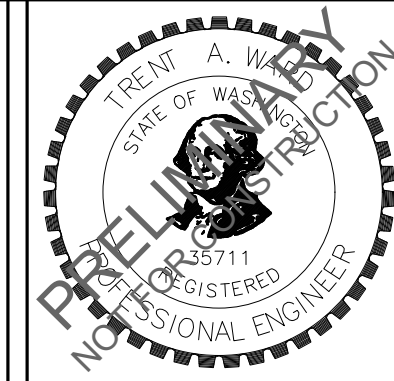
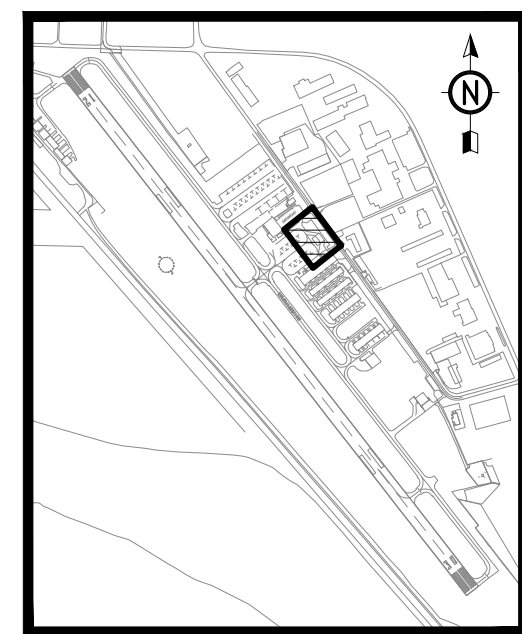
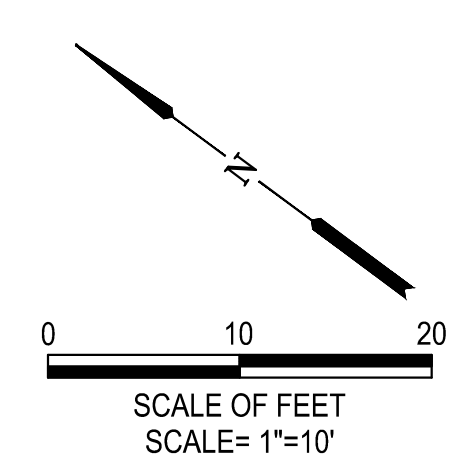


- LEGEND**
- PROPOSED CONTOUR LINE
  - - - - - EXISTING CONTOUR LINE
  - F - PROPOSED FILL LIMITS
  - - - - - GRADE BREAK LINE
  - CB PROPOSED TYPE 1 CATCH BASIN, SEE GENERAL NOTES 4, 5 AND DRAINAGE CONSTRUCTION NOTES
  - OWS PROPOSED OIL WATER SEPARATOR, SEE DRAINAGE CONSTRUCTION NOTES
  - ⊗ PROPOSED GATE VALVE

- GENERAL NOTES**
1. REFER TO SHEET C-05 FOR ADDITIONAL HORIZONTAL CONTROL.
  2. EXISTING PIPE SLOPES AND INVERTS ARE BASED ON THE BEST INFORMATION AVAILABLE. CONTRACTOR SHALL FIELD VERIFY INVERT ELEVATIONS AT ALL TIE IN POINTS PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN WHAT IS SHOWN IN THE PLANS AND ACTUAL SITE CONDITIONS.
  3. EXISTING PIPE TYPE, SIZE AND CONDITION ARE BASED ON THE BEST INFORMATION AVAILABLE. CONTRACTOR SHALL FIELD VERIFY TYPE, SIZE AND CONDITION PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS AND SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN WHAT IS SHOWN ON THE PLANS AND ACTUAL SITE CONDITIONS.

- GENERAL NOTES CONT.**
4. TYPE 1 CATCH BASINS SHALL MEET THE REQUIREMENTS OF WSDOT STANDARD PLAN B-5.20-03. TYPE 1 CATCH BASINS SHALL BE INSTALLED WITH A BOLTED DOWN RECTANGULAR SOLID METAL COVER PER WSDOT STANDARD PLAN B-30.20-04.
  5. PIPE EXTENSIONS, AS NECESSARY, TO CONNECT NEW TYPE 1 CATCH BASINS TO EXISTING PIPES SHALL BE SDR-35 PVC AND SHALL BE INCIDENTAL TO THE TYPE 1 CATCH BASIN.

- DRAINAGE CONSTRUCTION NOTES**
1. TRACER WIRE SHALL BE INSTALLED ALONG THE TOP OF ALL PIPE. EXTEND THE TRACER WIRE INTO THE CATCH BASINS AND ALL OTHER STRUCTURES, THEN UP THE INSIDE WALL OF STRUCTURES. PROVIDE 3 FEET OF COILED TRACER WIRE SLACK ATTACHED TO TOP INSIDE OF THE STRUCTURE.
  2. STORM SEWERS AND APPURTENANCES SHALL BE CLEANED, AIR TESTED, INFILTRATION TESTED AND DEFLECTION TESTED AFTER BACKFILLING AND PRIOR TO TOPSOILING AND SEEDING. TESTING SHALL BE COMPLETED PER THE REQUIREMENTS OF THE SPECIFICATIONS. TV INSPECTION SHALL BE PERFORMED AFTER CLEANING, TESTING AND CORRECTIONS ARE COMPLETE. AN ELECTRONIC COPY OF THE TV INSPECTION VIDEO AND THE TV INSPECTION REPORT SHALL BE PROVIDED TO THE ENGINEER. TOPSOIL AND SEEDING SHALL NOT BE PLACED UNTIL THE ENGINEER HAS APPROVED THE TV INSPECTION.
  3. ALL DRAINAGE STRUCTURES SHALL BE WATERTIGHT. FOLLOWING BACKFILL AND PRIOR TO FINAL TOPSOILING, PERFORM VACUUM TESTING ON STRUCTURES PER THE REQUIREMENTS OF THE SPECIFICATIONS.
  4. AS-BUILT DRAWINGS AND TV REPORTS SHALL BE PROVIDED PRIOR TO FINAL ACCEPTANCE.
  5. ALL REQUIREMENTS HEREIN SHALL BE CONSIDERED INCIDENTAL TO THE APPLICABLE BID ITEMS. NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE.



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ELLENSBURG OFFICE  
 208 W 9TH AVENUE, SUITE 3  
 ELLENSBURG, WA 98926  
 509.795.5870  
 509.639.2710 FAX

DATE: MAY 2023 PROJECT NO: 35005.010.02

DESIGNED BY: TAW  
 DRAWN BY: JCW  
 CHECKED BY: TAW  
 SCALE: AS NOTED

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
 TASK ORDER #5 - FINAL DESIGN FUEL SITE

GRADING & DRAINAGE PLAN

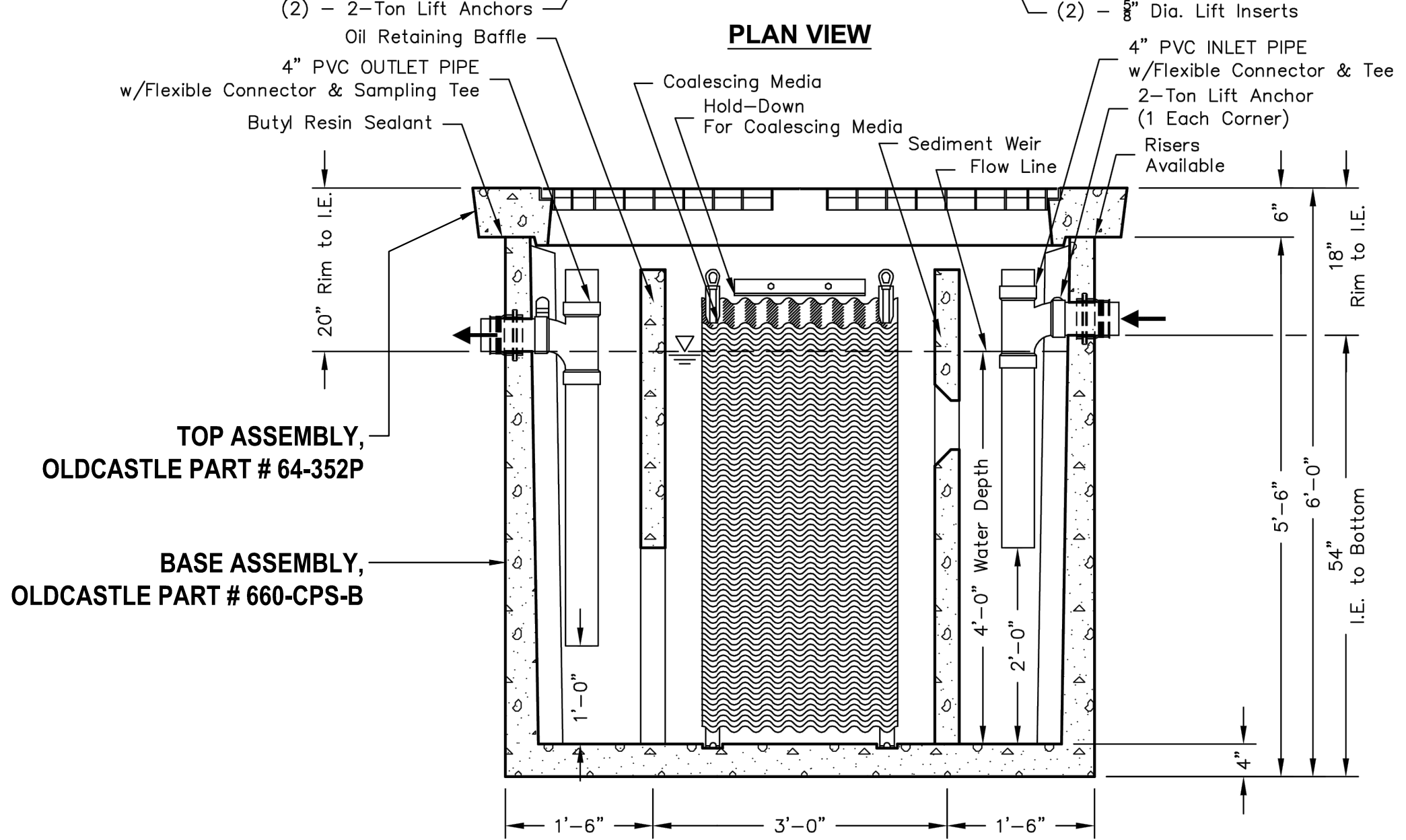
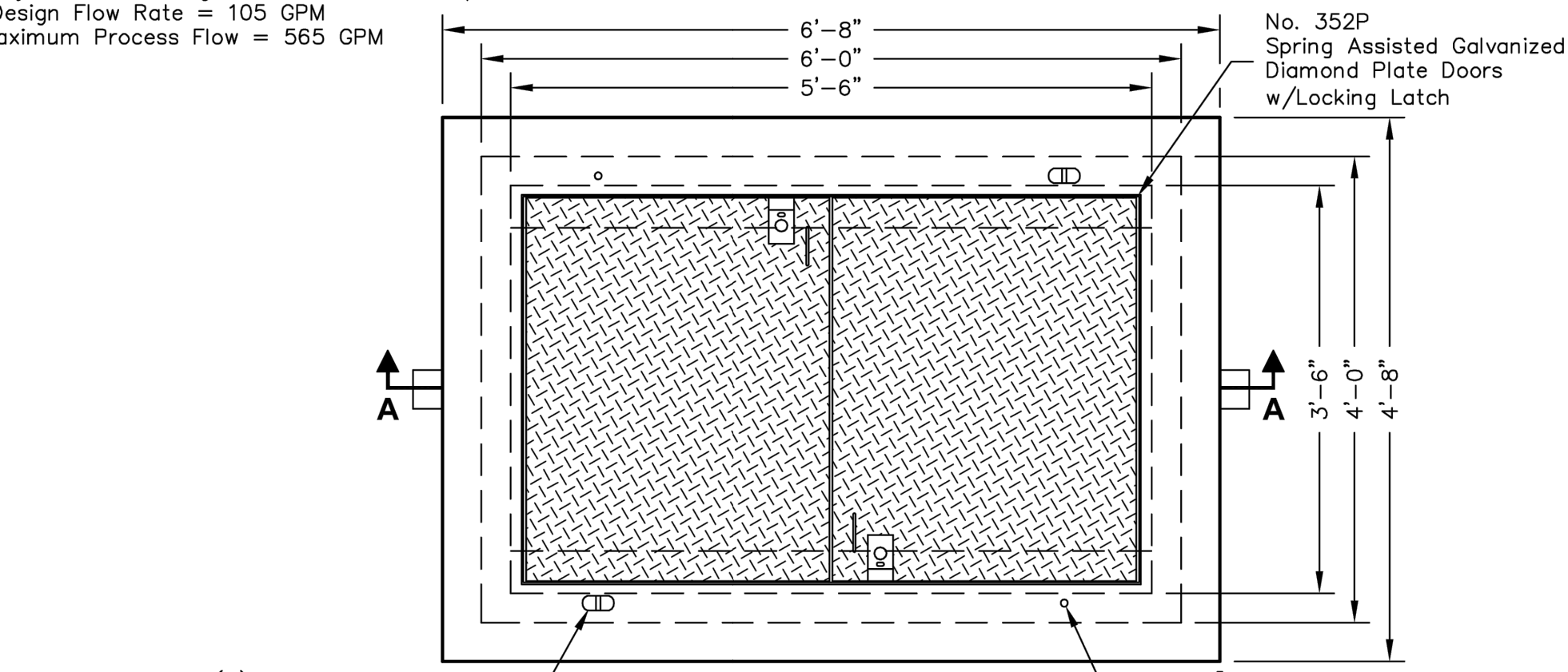
DRAWING NO. C-07  
 SHEET NO. 11 OF 22

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# 660-CPS

Projected Coalescing Plate Area = 444 Sq.Ft.  
 \*Design Flow Rate = 105 GPM  
 Maximum Process Flow = 565 GPM



*DESIGN FLOW RATE	EFFLUENT QUALITY	100% COLLECTED SIZE	Basic Design Information: *
105 GPM	10 ppm	60 Micron	Influent Characteristics - Oil Specific Gravity = 0.88 - Operating Temperature = 50° - Influent Oil Concentration = 100 ppm - Mean Oil Droplet Size = 130 Microns - .033 ft/min. Critical Oil Droplet Predicted Rise Rate

Notes:  
 - Static Water Depth = 4'-0"  
 - Prior to "Startup" of system, fill with clean water to bottom of outlet pipe. For best results, fill to flow line.  
 - Follow Regular Inspection, Cleaning, & Maintenance Schedule (See Clean Out & Maintenance).

\*Basic Design Information per Washington State Department of Ecology; User to Adjust Estimates for Variations in Real Conditions.

 PO Box 323, Wilsonville, Oregon 97070-0323 Tel: (503) 682-2844 Fax: (503) 682-2657	<b>660-CPS</b>	<b>660-CPS</b> <b>OIL / WATER SEPARATOR</b> <b>COALESCING - 105 GPM</b>
	File Name: 020-660CPS	
	Issue Date: 2018	
	oldcastleprecast.com/wilsonville	

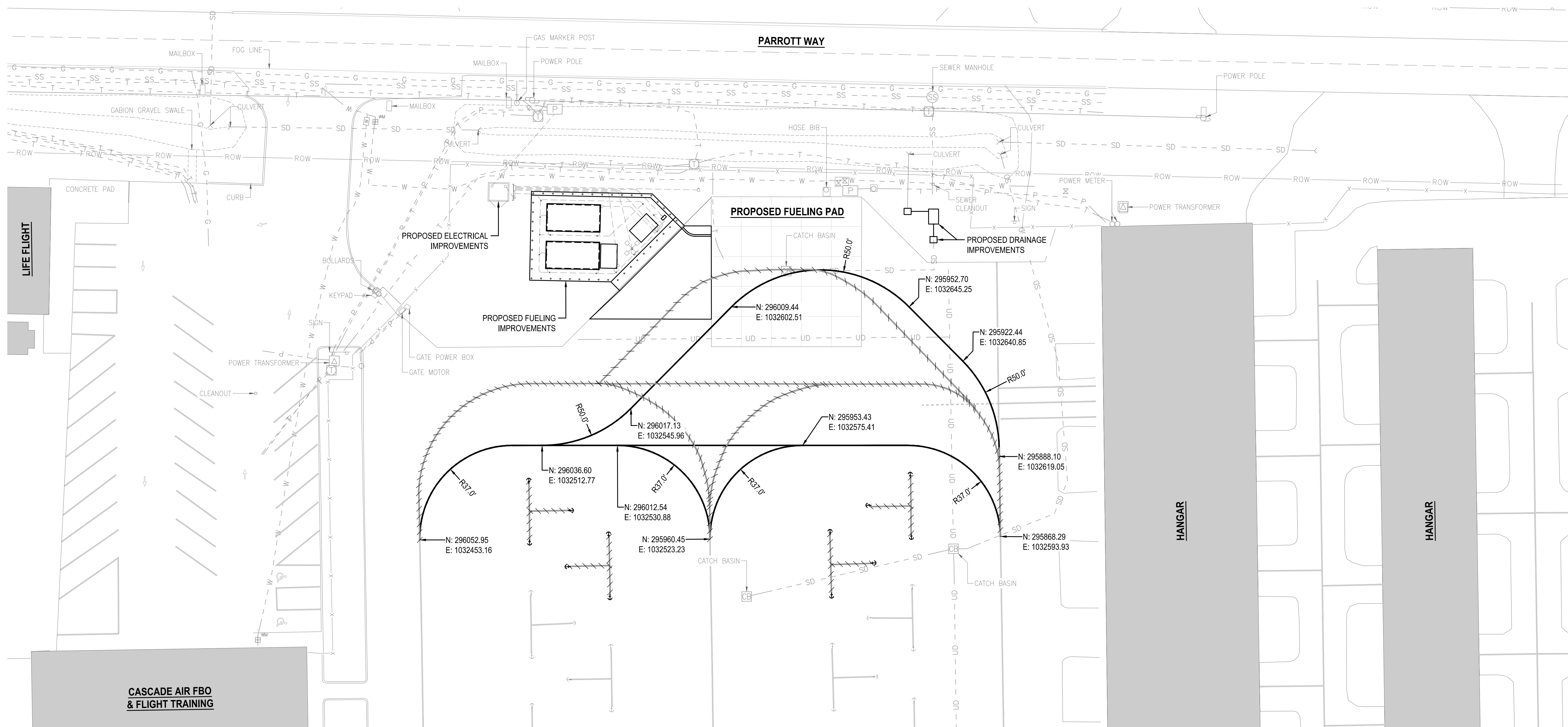
## 660-CPS OIL & WATER SEPARATOR

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C-08

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	 Know what's below. Call before you dig.		VERIFY SCALES BAR IS ONE INCH ON ORIGINAL DRAWING. 0" = 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>APPR</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DATE	BY	APPR	REVISIONS																					 ELLENSBURG OFFICE 208 W 9TH AVENUE, SUITE 3 ELLENSBURG, WA 98926 509.795.5870 509.639.2710 FAX	DESIGNED BY: TAW DRAWN BY: JCW CHECKED BY: TAW SCALE: AS NOTED	SOUTHWEST WASHINGTON REGIONAL AIRPORT TASK ORDER #5 - FINAL DESIGN FUEL SITE	DRAWING NO. <b>C-08</b> SHEET NO. <b>12 OF 22</b>
NO.	DATE	BY	APPR	REVISIONS																													
DATE: MAY 2023 PROJECT NO: 35005.010.02						DRAINAGE DETAILS																											



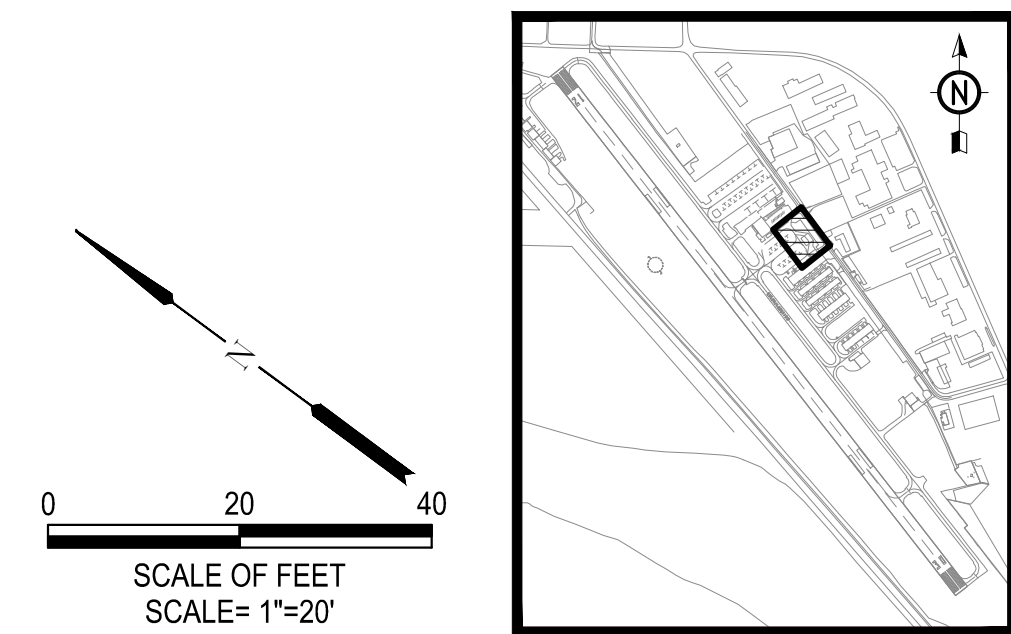


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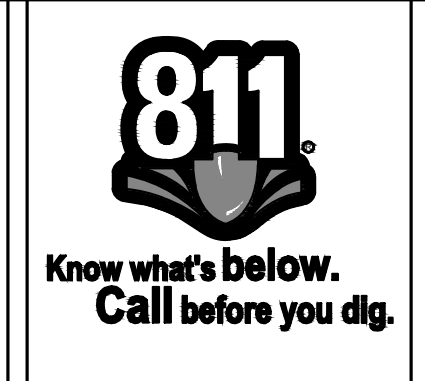
- PROPOSED 6" WIDE YELLOW CENTERLINE STRIPING, SEE GENERAL NOTE 1
- EXISTING 6" WIDE YELLOW STRIPING
- ////// STRIPING REMOVAL

**GENERAL NOTES**

1. ALL PROPOSED CENTERLINE STRIPING SHALL BE PROVIDED WITH A 6-INCH WIDE BLACK OUTLINE ON BOTH SIDES.



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 509.639.2710 FAX

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DESIGNED BY: DMY  
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 CHECKED BY: DEB  
 SCALE: AS NOTED

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
 TASK ORDER #5 - FINAL DESIGN FUEL SITE

**PAVEMENT MARKING REMOVAL & REPLACEMENT PLAN**

DRAWING NO. **C-09**  
 SHEET NO. **13 OF 22**

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DRAWING LIST		SYMBOL	DESCRIPTION	SYMBOL		DESCRIPTION	GENERAL NOTES
DRAWING NO.	SHEET TITLE			SCHEMATIC	PLAN		
E-01	ELECTRICAL SYMBOL AND LEGEND		DEVICE TERMINAL			GROUND ROD IN GROUND ROD BOX	1. "GENERAL NOTES" APPLY TO ALL DRAWINGS. "SHEET NOTES" APPLY TO ALL OF THE SHEETS ON WHICH THEY OCCUR. "KEYNOTES" APPLY ONLY WHERE CALLED OUT. 2. NOT ALL RACEWAYS REQUIRED ARE SHOWN ON THE DRAWINGS. REFERENCE CONDUIT AND WIRE SCHEDULES AND SPECIFICATIONS FOR REQUIRED RACEWAYS. FURNISH AND INSTALL ALL SCHEDULED RACEWAYS AND WIRE. 3. BRANCH CIRCUIT CONDUCTORS, NOT OTHERWISE IDENTIFIED SHALL BE A MINIMUM 12 AWG FOR RUNS 70 FEET OR LESS AND A MINIMUM 10 AWG FOR RUNS GREATER THAN 70 FEET. QUANTITY AND SIZE SHALL BE "AS REQUIRED" TO SERVE AND CONTROL DEVICE(S) OR EQUIPMENT WITH A MAXIMUM VOLTAGE DROP OF THREE PERCENT. WHERE CONTRACTOR CHOOSES TO RUN MORE THAN THREE CURRENT CARRYING CONDUCTORS WITHIN ONE RACEWAY OR CABLE, CONDUCTORS SHALL BE INCREASED IN SIZE TO COMPENSATE FOR THE DERATING REQUIRED PER NEC SECTION 310.15. CONDUCTOR AMPACITIES SHALL BE TAKEN FROM THE 75°C COLUMN. 4. MINIMUM CONDUIT IN EXTERIOR AND UNDERGROUND LOCATIONS TO BE 1". 5. CONTRACTOR SHALL PROVIDE CONDUIT AND WIRE FOR ALL CIRCUITS SHOWN ON DRAWINGS. 6. PROVIDE DEDICATED NEUTRAL CONDUCTOR FOR ALL BRANCH CIRCUITS. 7. ALL EQUIPMENT SHOWN IN BOLD LINEWEIGHT SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED. EXISTING ELECTRICAL EQUIPMENT, BACKGROUND AND/OR WORK/EQUIPMENT THAT IS TO BE PROVIDED BY OTHERS IS SHOWN IN LIGHTER LINEWEIGHTS. 8. THE SYMBOLS, ABBREVIATIONS AND NOTES ON THIS SHEET ARE INTENDED TO BE GENERAL AND COMPREHENSIVE AND DO NOT ALL APPLY TO THIS PROJECT.
E-02	ELECTRICAL SITE PLAN		TERMINAL IN CONTROL PANEL			BATTERY	
E-03	ELECTRICAL GROUNDING PLAN		TERMINAL AT REMOTE DEVICE OR PANEL			TRANSFORMER, PLAN VIEW SHOWN TO SCALE	
E-04	ELECTRICAL ONE-LINE DIAGRAM AND SCHEDULES		THERMAL OVERLOAD RELAY			CURRENT TRANSFORMER, NUMBER INDICATES NUMBER OF C.T.'S. PLAN VIEW SHOWN TO SCALE	
E-05	ELECTRICAL DETAILS		SOLID STATE OVERLOAD			MOTOR, NUMBER INDICATES HORSEPOWER	
E-06	ELECTRICAL WIRING DIAGRAMS		MOTORIZED LOUVER			ELECTRIC HEATER WINDING, WATTAGE INDICATED	
			EXHAUST FAN			GENERATOR, PLAN VIEW SHOWN TO SCALE	
			CONDUIT CONCEALED IN WALL, CEILING, UNDER FLOOR, IN FLOOR SLAB, OR ROUTED UNDERGROUND			SOLENOID VALVE	
			CONDUIT EXPOSED			FULL VOLTAGE NON-REVERSING STARTER/NEMA SIZE MS = MOTOR STARTER CONTACT BP = BYPASS CONTACTOR IC = ISOLATION CONTACTOR FVNR = FULL VOLTAGE NON-REVERSING	
			EXISTING CONDUIT ROUTED UNDERGROUND			DISCONNECT SWITCH, NON FUSED (60A) INDICATES AMPERAGE RATING	
			OVERHEAD ELECTRICAL			DISCONNECT SWITCH, FUSED 200=SWITCH RATING, 100=FUSE RATING	
			EXTRA-HARD USAGE FLEXIBLE CORD			UTILITY WATT HOUR METER	
			CONDUIT FLEXIBLE			INDICATING LIGHT: A = AMBER G = GREEN W = WHITE B = BLUE R = RED	
			ELECTRICAL HEAT TRACE CABLE			LIGHTED PUSHBUTTON	
			CONDUIT TURNED UP OR TOWARD			SELECTOR SWITCH: HOR = HAND/OFF/REMOTE HOA = HAND/OFF/AUTO RO = RUN/OFF	
			CONDUIT TURNED DOWN OR AWAY			PUSHBUTTON SWITCH, MOMENTARY ON	
			CONDUIT CAPPED			PRESSURE SWITCH, NORMALLY CLOSED	
			CONDUIT HOME RUN 3/4", 2#12 & 1#12 GND. UNLESS SHOWN OTHERWISE. (EXAMPLE SHOWN: TO PANEL P1, CIRCUIT 1)			FLOW SWITCH, NORMALLY CLOSED	
			HANDHOLE WITH DESIGNATION			LIMIT SWITCH, NORMALLY OPEN	
			JUNCTION BOX			LEVEL SWITCH, CLOSSES ON RISING LEVEL	
			CKT. BKR, RATING/NO. OF POLES WITH THERMAL MAGNETIC CIRCUIT BREAKER TRIP			TS, TEMP. SWITCH, CLOSSES ON FALLING TEMP T, THERMOSTAT	
			MANUAL OR AUTOMATIC TRANSFER SWITCH			TIMED CONTACT, CONTACT ACTION IS RETARTED AFTER COIL IS ENERGIZED - NOTC	
			POWER CAPACITOR			TIMED CONTACT, CONTACT ACTION IS RETARTED AFTER COIL IS ENERGIZED - NCTO	
			VARIABLE FREQUENCY DRIVE (XXA INDICATES CURRENT RATING)			TIMED CONTACT, CONTACT ACTION IS RETARTED AFTER COIL IS DEENERGIZED - NCTO	
			SOLID STATE STARTER, REDUCED VOLTAGE WITH INTEGRAL & BYPASS CONTACTORS (XXA INDICATES CURRENT RATING)			TIMED CONTACT, CONTACT ACTION IS RETARTED AFTER COIL IS DEENERGIZED - NCTC	
			FUSE			POTENTIOMETER	
			DIGITAL METERING SYSTEM			LT = LEVEL TRANSMITTER	
			CR = CONTROL RELAY			PT = PRESSURE TRANSMITTER	
			TDR = TIME DELAY RELAY			SI = SPEED INDICATOR	
			TR = TIMER RELAY			FT = FLOW TRANSMITTER	
			PHASE FAIL RELAY & FUSE			VB = VIBRATION TRANSMITTER	
			RUN TIME METER			TT = TEMPERATURE TRANSMITTER	
			PHOTO ELECTRIC CELL			FI = FLOW INDICATOR	
			CONDUCTORS NOT CONNECTED			DOOR SWITCH	
			CONDUCTORS CONNECTED			OVERTEMPERATURE CUTOUT	
			PULL OUT SWITCH/PLUG-RECEPTACLE CONNECTION			LOCAL EQUIPMENT CONTROL PANEL - MCP, LCP, FACP	
			HORN			UNIT HEATER	
			SEAL OFF				
			INTERCOM STATION				

SYMBOL	DESCRIPTION
	LIGHTING FIXTURE, SURFACE "L1" INDICATES TYPE PER LUMINAIRE SCHEDULE "P1-2" INDICATES CIRCUITING "a" INDICATES SWITCHING
	LIGHTING FIXTURE, RECESSED
	STRIP, SURFACE OR PENDANT AS INDICATED IN LUMINAIRE SCHEDULE
	LIGHTING FIXTURE, EMERGENCY
	LIGHTING FIXTURE, CEILING MOUNTED
	FIXTURE, WALL MOUNT
	EXIT LIGHT, ↓ INDICATES DIRECTION OF ARROW
	EMERGENCY WALL PACK
	LIGHTING FIXTURE, POLE MOUNTED
	SURFACE METAL RACEWAY WIRECEPTACLE @ X" O.C.
	DOUBLE DUPLEX RECEPTACLES
	DUPLEX RECEPTACLE P1-4= CIRCUIT NUMBER (TYP) GFCI= CLASS A GROUND FAULT CIRCUIT INTERRUPTER WP= WEATHER PROOF XP= EXPLOSION PROOF
	SPECIAL PURPOSE RECEPTACLE
	THERMOSTAT
	SMOKE DETECTOR
	HEAT DETECTOR
	DATA OUTLET
	TELEPHONE OUTLET
	COMBINATION DATA/TELEPHONE OUTLET
	SINGLE POLE SWITCH
	2 = DOUBLE POLE SWITCH 3 = THREE WAY SWITCH 4 = FOUR WAY SWITCH P = SWITCH AND PILOT LAMP K = KEY OPERATED SWITCH M = MOTOR RATED SWITCH (FOR USE WITH THERMALLY PROTECTED MOTORS)
	WP = WEATHER PROOF SWITCH T = SWITCH WITH TIMER
	PANELBOARD
	HUMIDISTAT
	OCCUPANCY SENSOR
	EQUIPMENT TO BE REMOVED

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ABBREVIATIONS			
AFC	AVAILABLE FAULT CURRENT	GFCI	GROUND FAULT CIRCUIT INTERRUPTER
AFG	ABOVE FINISHED GRADE	GFP	GROUND FAULT PROTECTION
AF	AMP FRAME	GND	GROUND
AFCI	ARC FLASH CIRCUIT INTERRUPTER	GRS	GALVANIZED RIGID STEEL CONDUIT
AFF	ABOVE FINISHED FLOOR	HH	HANDHOLE
AI	ANALOG INPUT POINT	HOA	HAND-OFF-AUTO
AIC	AMPERE INTERRUPTING CAPACITY	HSS	HOLLOW STRUCTURAL SECTIONS
AHJ	AUTHORITY HAVING JURISDICTION	IC	ISOLATION CONTACT
AO	ANALOG OUTPUT POINT	IMC	INTERMEDIATE METALLIC CONDUIT
ATS	AUTOMATIC TRANSFER SWITCH	ISB	INTRINSICALLY SAFE BARRIER
BC	BATTERY CHARGER	ISR	INTRINSICALLY SAFE RELAY
BH	BLOCK HEATER	JBOX	JUNCTION BOX
C	CONDUIT	LCP	LIGHTING CONTROL PANEL
CB	CIRCUIT BREAKER	LTG	LIGHTING
COMM	COMMUNICATIONS	MCC	MOTOR CONTROL CENTER
CPT	CONTROL POWER TRANSFORMER	MDP	MAIN DISTRIBUTION PANEL
CT	CURRENT TRANSFORMER	MLO	MAIN LUGS ONLY
DI	AC DIGITAL INPUT POINT	NL	NIGHT LIGHT
DO	AC DIGITAL OUTPUT POINT	NTS	NOT TO SCALE
EC	ELECTRICAL CONTRACTOR	OC	ON CENTER
EF	EXHAUST FAN	OH	OVERHEAD
EMT	ELECTRICAL METALLIC TUBING	OIT	OPERATOR INTERFACE TERMINAL
FAAP	FIRE ALARM ANNUNCIATOR PANEL	OS	OCCUPANCY SENSOR
FACP	FIRE ALARM CONTROL PANEL	PB	PUSH BUTTON
PIV	POST INDICATOR VALVE	PNL	PANEL
PS	POTENTIAL TRANSFORMER	PT	PUSH TO TEST
PVC	POLYVINYL CHLORIDE	RCPT	RECEPTACLE
RMC	RIGID METAL CONDUIT	RTM	RUN TIME METER
SA	SURGE ARRESTOR	SS	STAINLESS STEEL
SPD	SURGE PROTECTIVE DEVICE	STP	SHIELDED TWISTED PAIR
SV	SOLENOID VALVE	SV	SOLENOID VALVE
TYP	TYPICAL	UH	UNIT HEATER
UL	UNDERWRITERS LABORATORIES	UL	UNDERWRITERS LABORATORIES
UG	UNDERGROUND	UG	UNDERGROUND
UNON	UNLESS OTHERWISE NOTED	UTP	UNSHIELDED TWISTED PAIR
VFD	VARIABLE FREQUENCY DRIVE	VP	WEATHERPROOF
XFMR	TRANSFORMER	XFMR	TRANSFORMER

REFERENCE SYMBOLS			
	G1	GROUNDING ELECTRODE SYSTEM CONDUIT & WIRE TAG	
	P1	POWER CONDUIT & WIRE TAG	
	C1	CONTROL CONDUIT & WIRE TAG	
	S1	SIGNAL CONDUIT & WIRE TAG	
	T1	TELEPHONE CONDUIT & WIRE TAG	
	R1	SPARE CONDUIT & WIRE TAG	
	XX	FAULT CURRENT TAG (AIC)	
	X	KEY NOTE	
	P-01	MECHANICAL EQUIP. DESIGNATION	
	PSL-101	INSTRUMENT DESIGNATION	

SECTION LETTER SCALE			
	A	SECTION LETTER	
	XXX	SECTION LETTER	
	XXX	SHEET WHERE SECTION IS TAKEN FROM	
	XXX	SHEET WHERE SECTION APPEARS	

TITLE SCALE			
	1	DETAIL NUMBER	
	XXX	DETAIL NUMBER	
	XXX	SHEET WHERE DETAIL IS TAKEN FROM	
	XXX	SHEET ON WHICH DETAIL APPEARS	

DETAIL AREA			
	1	DETAIL NUMBER	
	XXX	DETAIL NUMBER	
	XXX	SHEET WHERE DETAIL IS TAKEN FROM	
	XXX	SHEET ON WHICH DETAIL APPEARS	

			VERIFY SCALES BAR IS ONE INCH ON ORIGINAL DRAWING. 0" = 1" IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	NO. DATE BY APPR REVISIONS		DESIGNED BY: GJW DRAWN BY: JTR CHECKED BY: GJW SCALE: AS NOTED	SOUTHWEST WASHINGTON REGIONAL AIRPORT TASK ORDER #5 - FINAL DESIGN FUEL SITE	DRAWING NO. E-01
			ELLensburg OFFICE 208 W 9TH AVENUE, SUITE 3 ELLensburg, WA 98926 509.795.5870 509.639.2710 FAX	DATE: MAY 2023 PROJECT NO: 35005.010.02	ELECTRICAL SYMBOL AND LEGEND	SHEET NO. 14 OF 22		

**PERMIT SET**

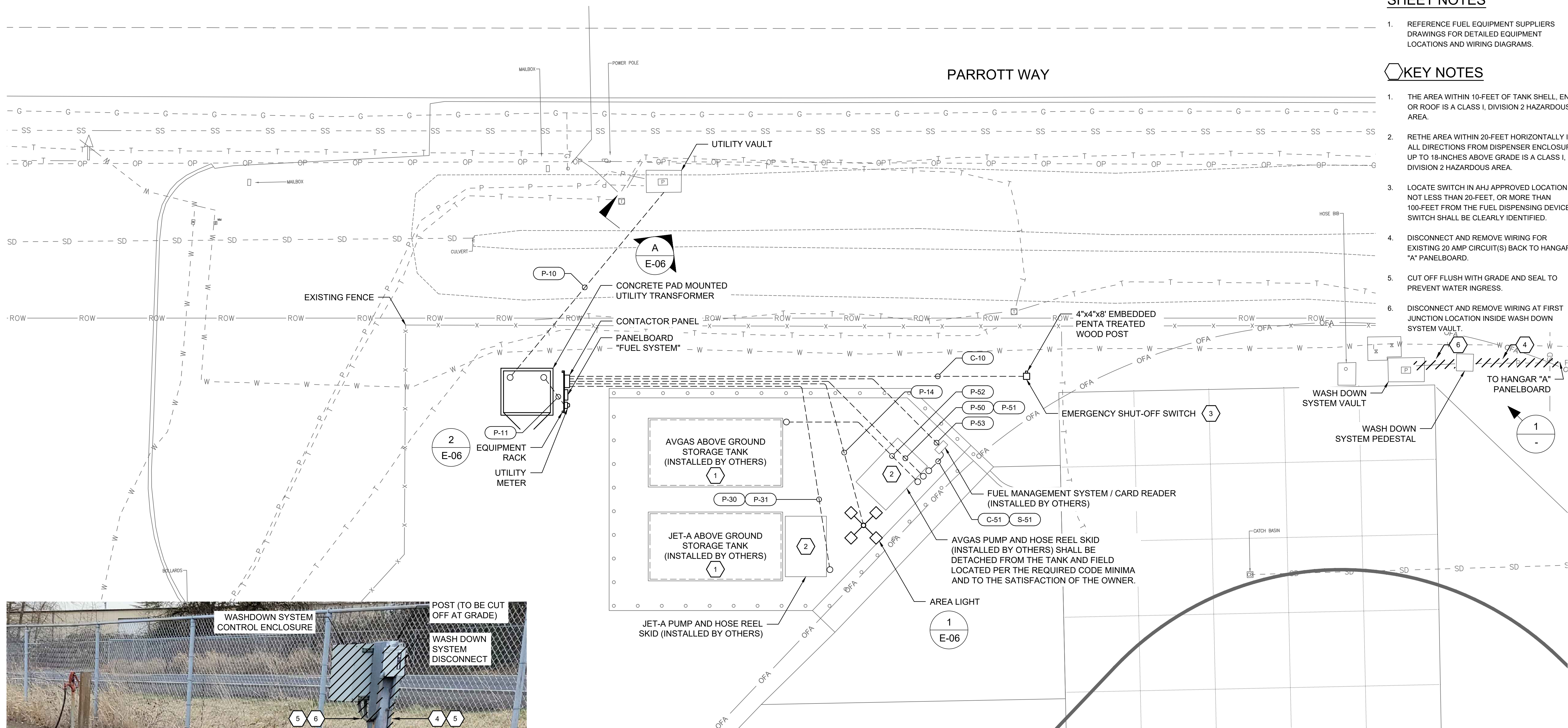


**SHEET NOTES**

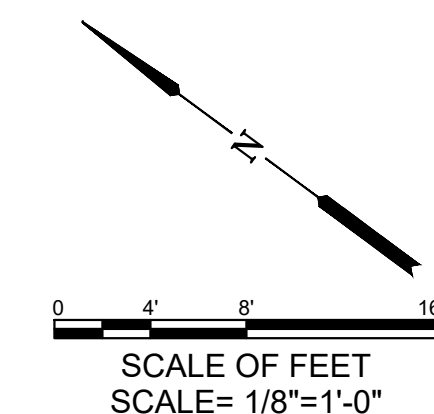
1. REFERENCE FUEL EQUIPMENT SUPPLIERS DRAWINGS FOR DETAILED EQUIPMENT LOCATIONS AND WIRING DIAGRAMS.

**KEY NOTES**

1. THE AREA WITHIN 10-FEET OF TANK SHELL, ENDS, OR ROOF IS A CLASS I, DIVISION 2 HAZARDOUS AREA.
2. RETHE AREA WITHIN 20-FEET HORIZONTALLY IN ALL DIRECTIONS FROM DISPENSER ENCLOSURE, UP TO 18-INCHES ABOVE GRADE IS A CLASS I, DIVISION 2 HAZARDOUS AREA.
3. LOCATE SWITCH IN AHJ APPROVED LOCATION NOT LESS THAN 20-FEET, OR MORE THAN 100-FEET FROM THE FUEL DISPENSING DEVICES. SWITCH SHALL BE CLEARLY IDENTIFIED.
4. DISCONNECT AND REMOVE WIRING FOR EXISTING 20 AMP CIRCUIT(S) BACK TO HANGAR "A" PANELBOARD.
5. CUT OFF FLUSH WITH GRADE AND SEAL TO PREVENT WATER INGRESS.
6. DISCONNECT AND REMOVE WIRING AT FIRST JUNCTION LOCATION INSIDE WASH DOWN SYSTEM VAULT.



1  
-  
NTS  
**WASHDOWN SYSTEM PEDESTAL DEMOLITION PHOTO DETAIL**



**PERMIT SET**



**PRELIMINARY**  
NOT FOR CONSTRUCTION

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208 W 9TH AVENUE, SUITE 3  
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509.795.5870  
509.639.2710 FAX

DESIGNED BY:  
GJW  
DRAWN BY:  
JTR  
CHECKED BY:  
GJW  
SCALE:  
AS NOTED

DATE: MAY 2023 PROJECT NO: 35005.010.02

**SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE**

**ELECTRICAL SITE PLAN**

DRAWING NO.  
**E-02**  
SHEET NO.  
**15 OF 22**









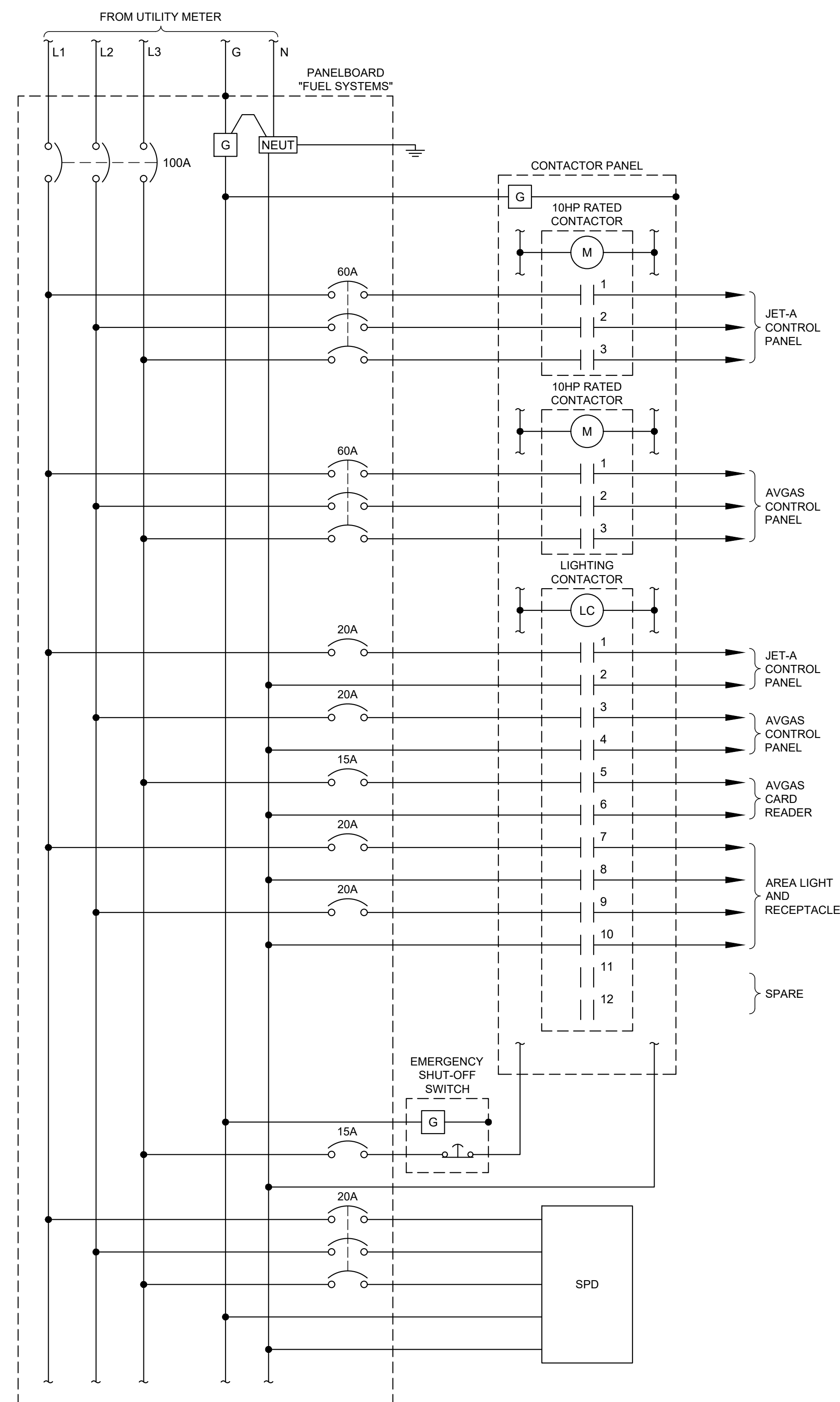






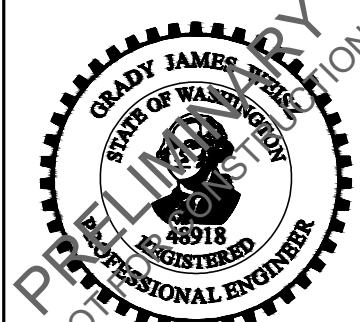
**SHEET NOTES**

1. PANEL WIRING SHOWN IS SCHEMATIC IN NATURE AND IS INTENDED TO CONVEY FUNCTIONAL INTENT.



**CONTACTOR PANEL WIRING DIAGRAM**

**PERMIT SET**



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 0" ——— 1"  
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DESIGNED BY: GJW	SCALE: AS NOTED
DRAWN BY: JTR	
CHECKED BY: GJW	
DATE: MAY 2023	

PROJECT NO:  
35005.010.02

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
 TASK ORDER #5 - FINAL DESIGN FUEL SITE

ELECTRICAL WIRING DIAGRAMS

DRAWING NO.  
**E-06**  
 SHEET NO.  
19 OF 22

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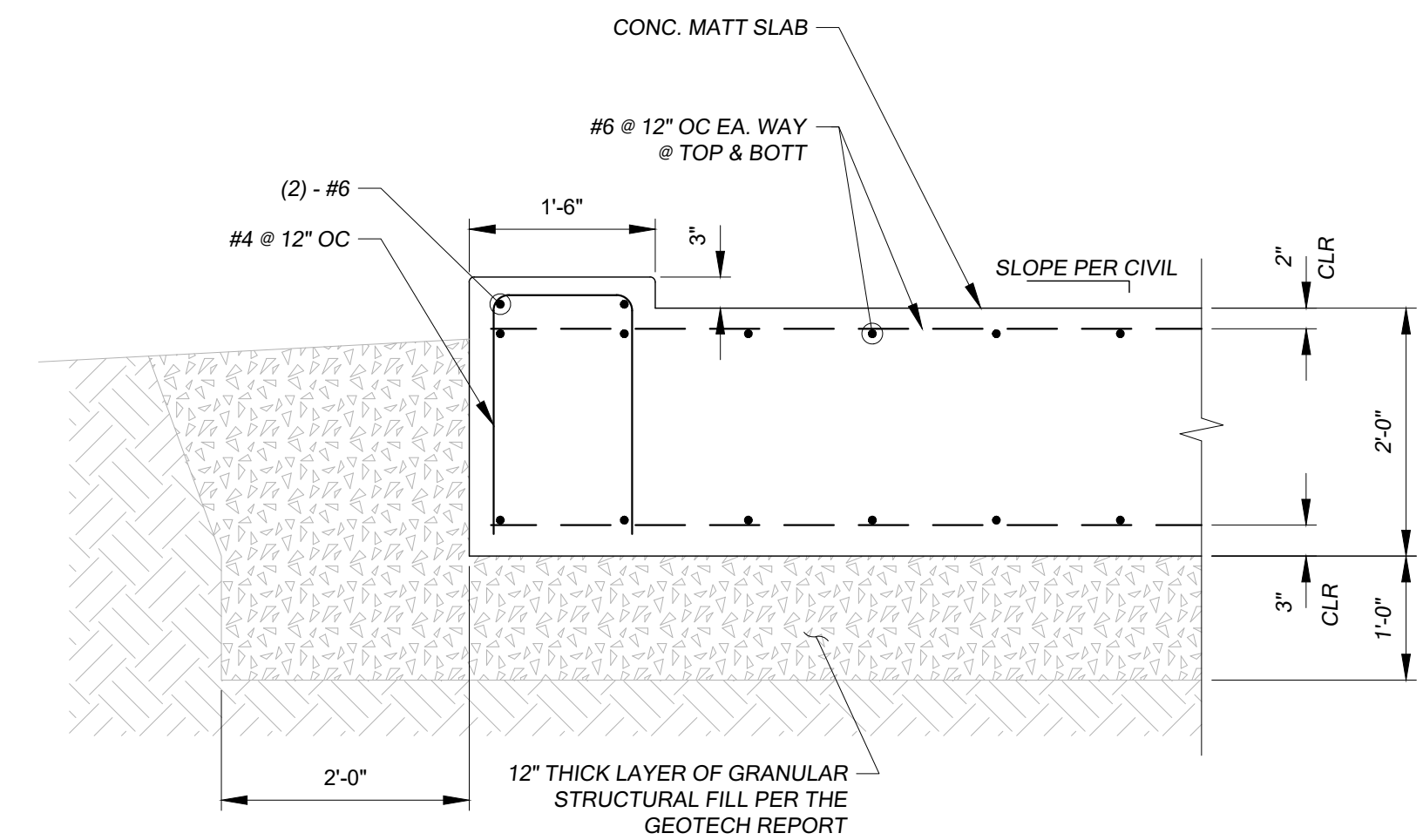


BAR SIZE	MISCELLANEOUS BARS			TOP BARS (see note #4)		
	Ld	CLASS A SPLICE	CLASS B SPLICE	Ld	CLASS A SPLICE	CLASS B SPLICE
f <sub>c</sub> = 4000psi						
#3	17	17	22	22	22	28
#4	22	22	29	29	29	37
#5	27	27	36	36	36	46
#6	33	33	43	43	43	56
#7	48	48	62	62	62	81

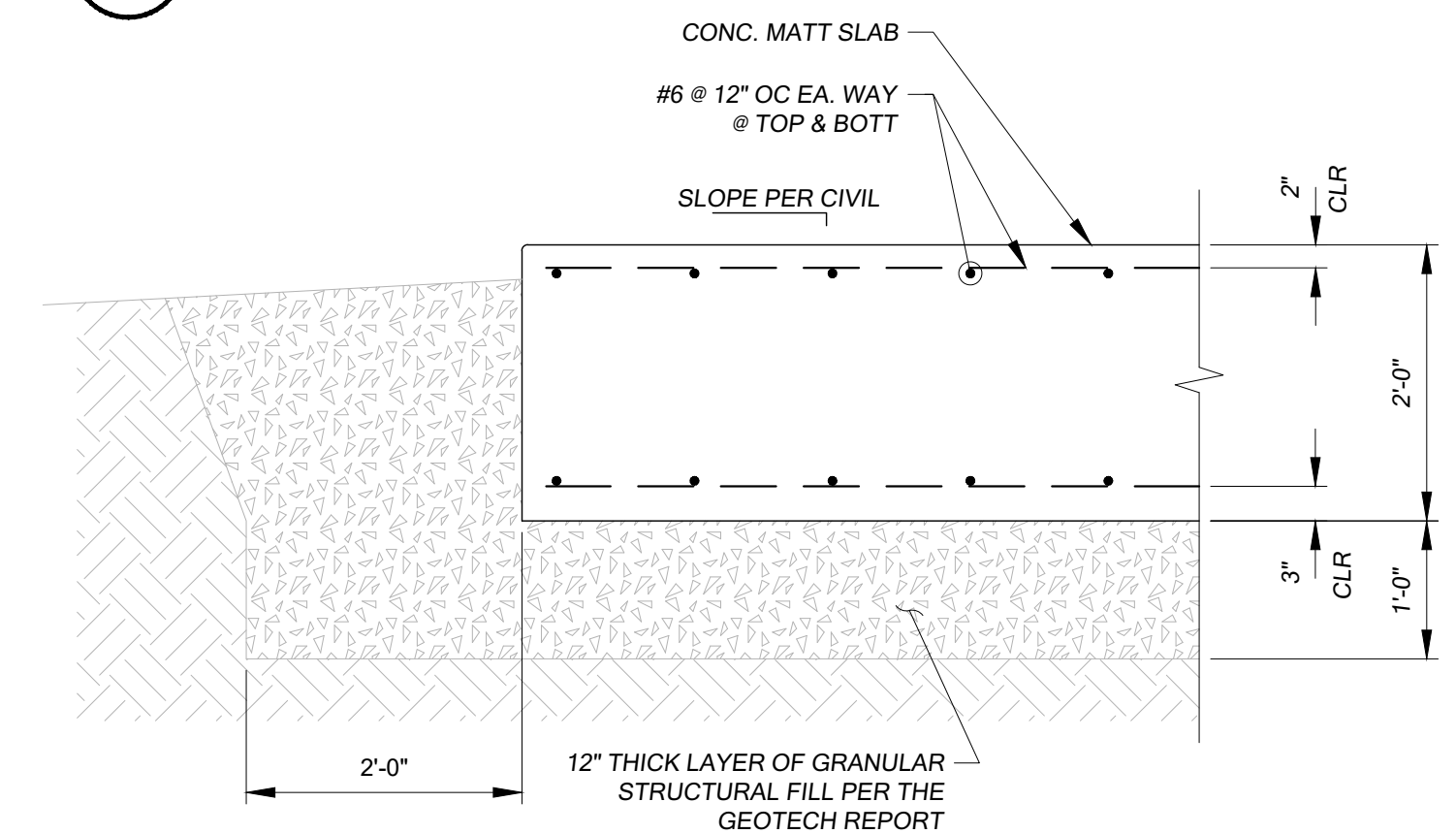
**NOTE:**

- VALUES FOR UNCOATED REINFORCING AND NORMAL WEIGHT CONCRETE WITH CLEAR SPACING > db, CLEAR COVER > db.
- DEVELOP ALL REINFORCING IN STRUCTURAL SLABS WITH MINIMUM DEVELOPMENT LENGTH L<sub>d</sub>.
- TOP BAR IS DEFINED AS HORIZONTAL BARS WITH MORE THAN 12" OF FRESH CONCRETE BELOW OR AS NOTED ON DOCUMENTS AS "TOP BAR".
- UNLESS NOTED OTHERWISE, ALL LAPS SHALL BE A MINIMUM CLASS B OR CLASS B (TOP BARS).
- ALL TABULATED VALUES ARE IN INCHES.

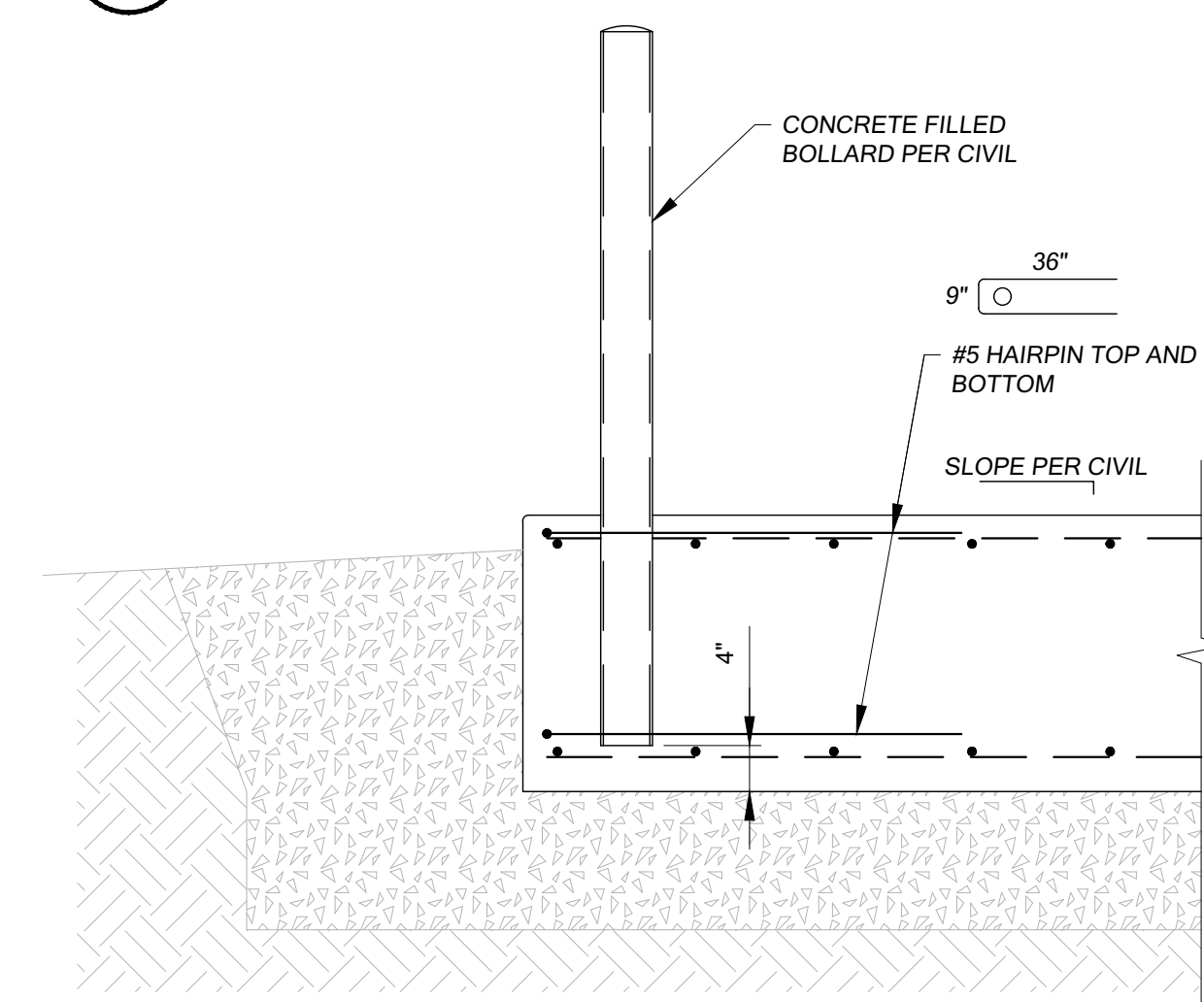
**1 TYPICAL LAP SPLICE SCHEDULE**  
SCALE: NONE



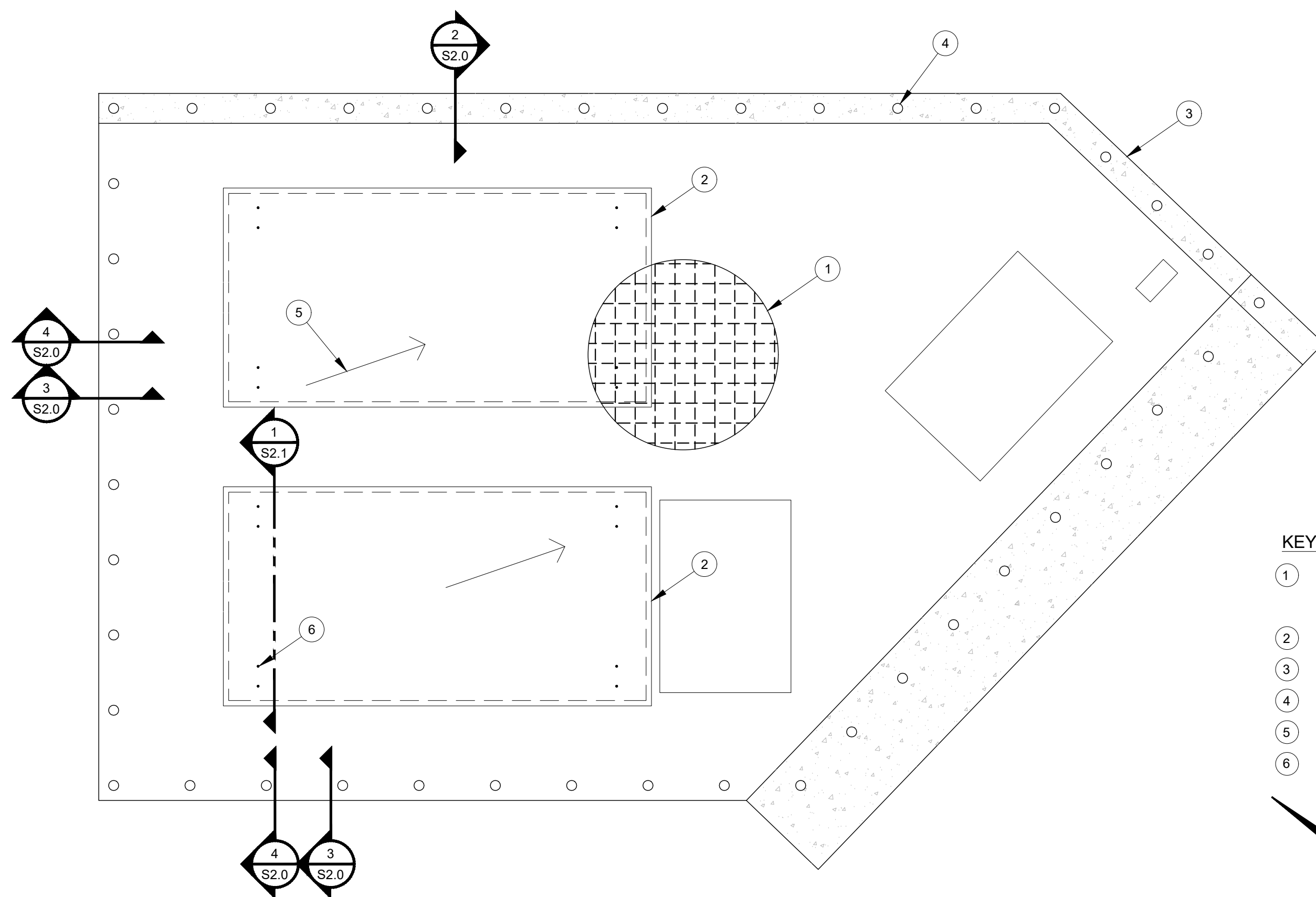
**2 TYPICAL MAT SLAB DETAIL**  
SCALE: 3/4"=1'-0"



**3 TYPICAL MAT SLAB DETAIL**  
SCALE: 3/4"=1'-0"

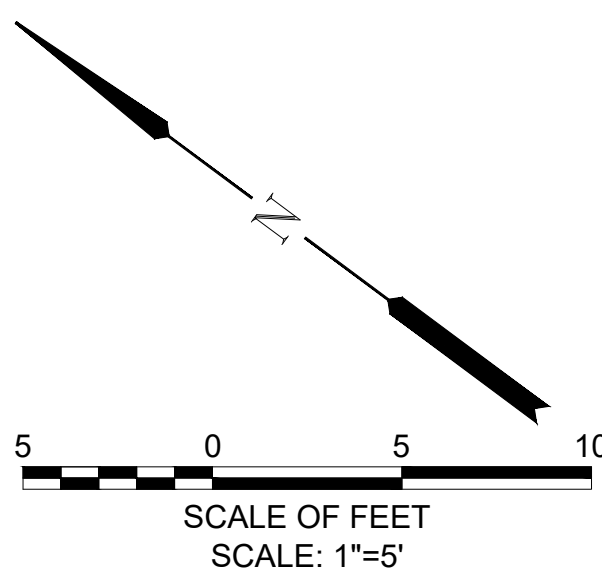


**4 REINFORCING AT BOLLARD**  
SCALE: 3/4"=1'-0"



**KEY NOTES:**

- 24" THICK CONCRETE SLAB WITH #6 REBAR @ 12" OC EACH WAY TOP AND BOTTOM. REFER TO CIVIL PLAN FOR DIMENSIONING AND TANK LOCATIONS
- 12,000 GALLON FUEL TANK
- CONCRETE CURB PER CIVIL
- STEEL BOLLARD PER CIVIL
- SLOPE PER CIVIL
- POST INSTALLED ANCHORS



**MAT FOUNDATION PLAN**

**PERMIT SET**



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0" = 1"  
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

NO.	DATE	BY	APPR	REVISIONS
▲	xxx	xxx	xxx	xxx



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ELLENSBURG, WA 98926  
509.795.5870  
509.639.2710 FAX

DATE: 06.27.2023

PROJECT NO: MDS No. 3385

DESIGNED BY: MJD  
DRAWN BY: MJD  
CHECKED BY: BK  
SCALE: SHOWN

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

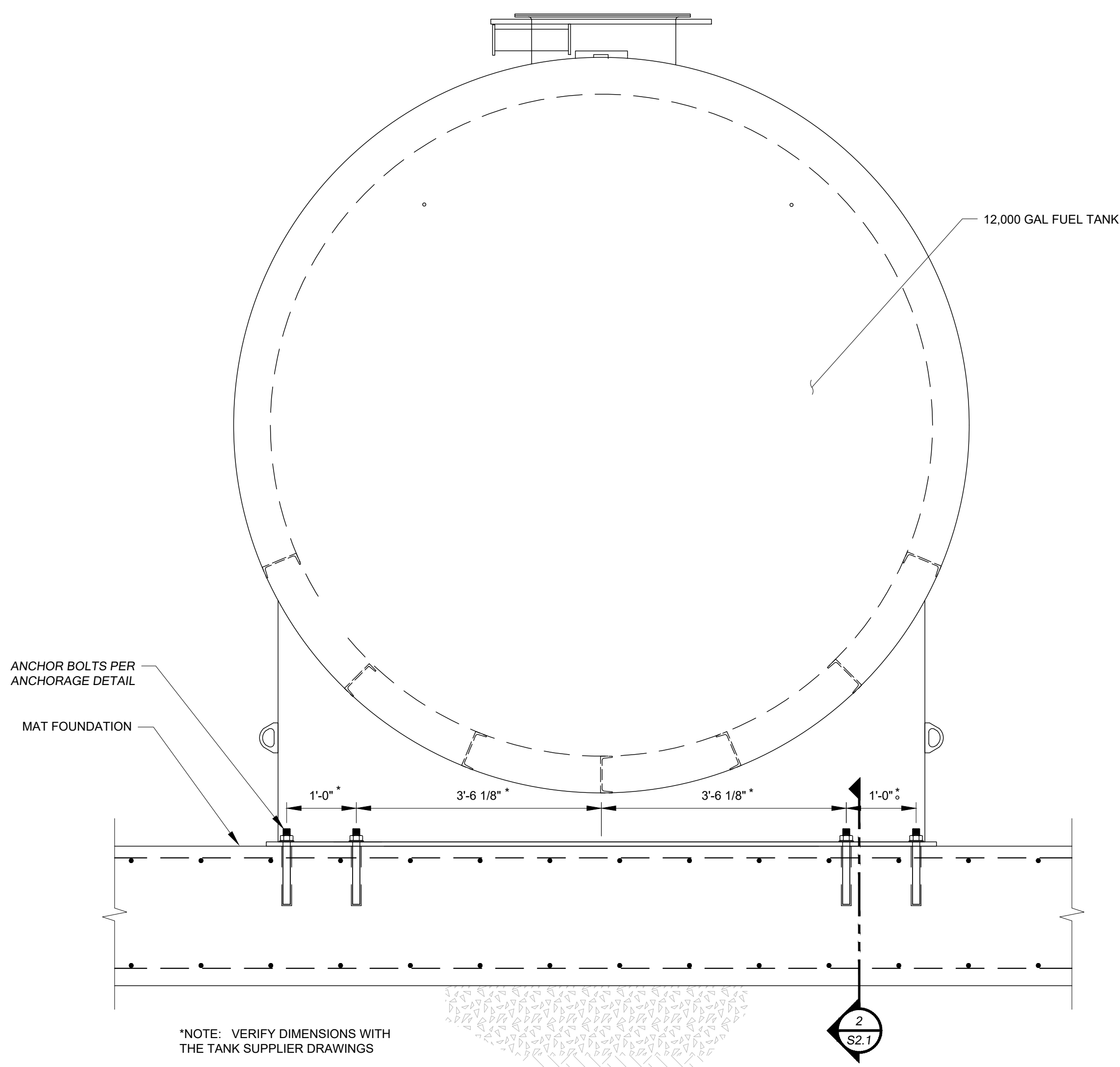
MAT FOUNDATION

DRAWING NO. S2.0

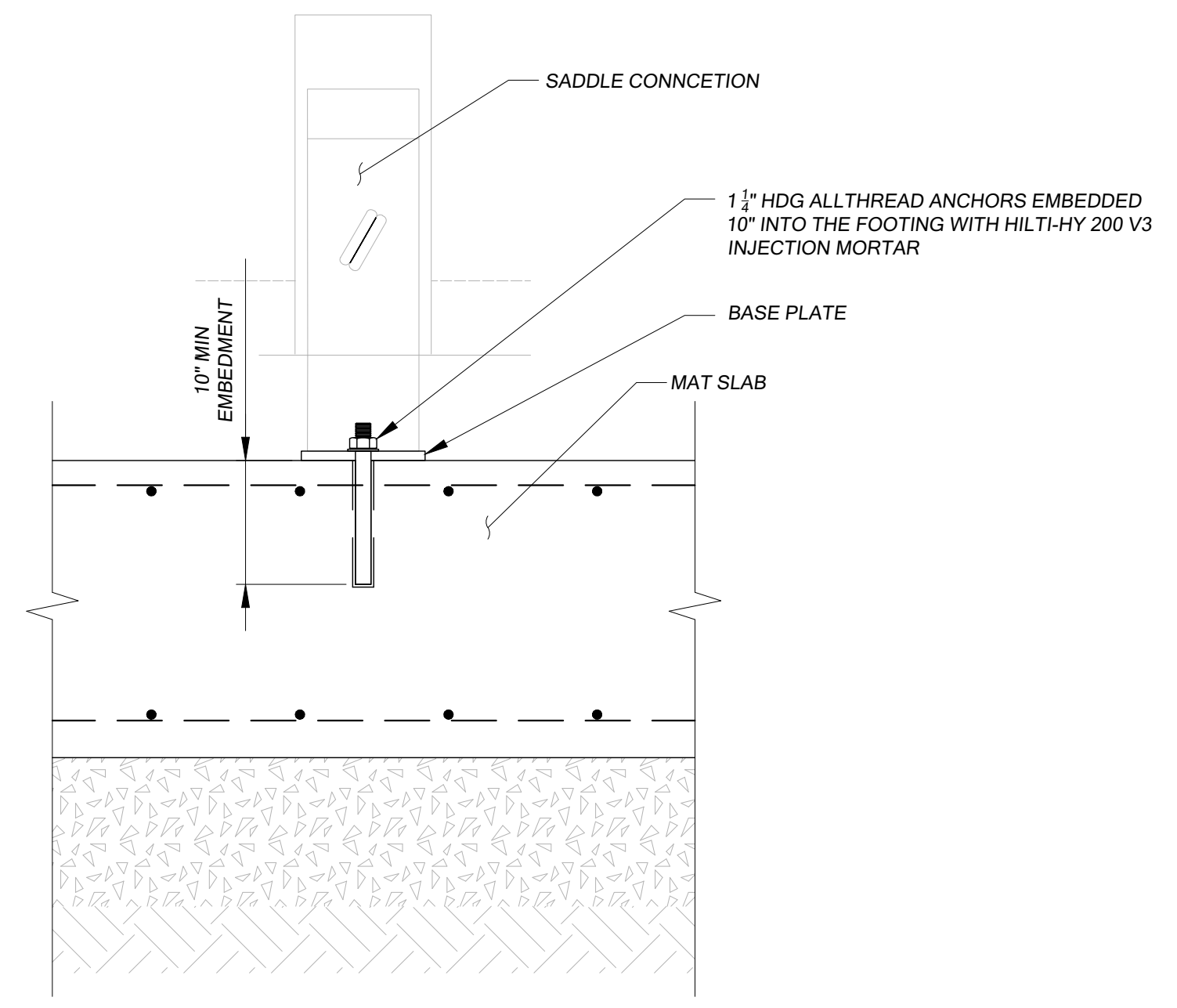
SHEET NO. 21 OF 22

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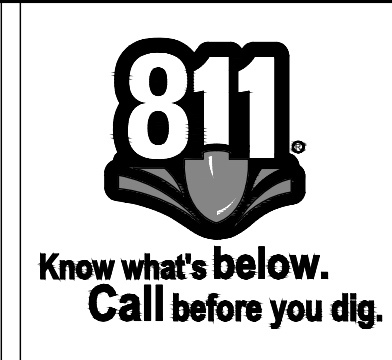


**1 TANK ANCHORAGE SECTION**  
SCALE: 3/4" = 1'-0"



**2 TANK ANCHORAGE DETAIL**  
SCALE: NONE

**PERMIT SET**



VERIFY SCALES  
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NO.	DATE	BY	APPR	REVISIONS
▲	XXX	XXX	XXX	XXX



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CHECKED BY: BK  
SCALE: SHOWN

SOUTHWEST WASHINGTON REGIONAL AIRPORT  
TASK ORDER #5 - FINAL DESIGN FUEL SITE

MAT FOUNDATION

DRAWING NO. S2.1  
SHEET NO. 22 OF 22

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